



Wayzata Boulevard Corridor Study

October 2023

ACKNOWLEDGEMENTS AND TABLE OF CONTENTS

ACKNOWLEDGEMENTS

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PART A: CONTEXT

PART A: CONTEXT

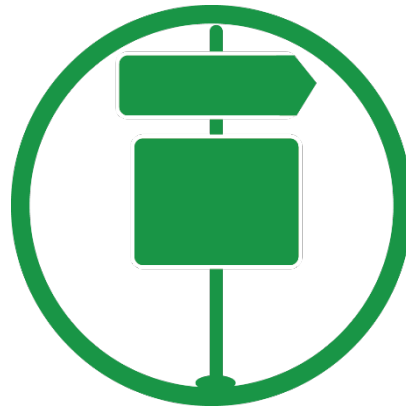
INTRODUCTION

Wayzata Boulevard is a major roadway within Wayzata, connecting thriving retail areas and welcoming neighborhoods. As an active and vibrant corridor, Wayzata Boulevard contributes to the community's sense of identity. Future development and transportation investments will shape the transportation experience and surrounding community for years to come.

Overall, the corridor study set out to explore opportunities and strategies for the future of Wayzata Boulevard and Central Avenue in four areas.



Land Use &
Development



Safety
Improvements



Transportation &
Mobility



Public Realm

PROJECT OVERVIEW

A Corridor Study Committee guided the overall development of the planning process. The Committee was made up of the members of the City Council, Planning Commission and Housing and Redevelopment Authority (HRA). This group actively worked together to define solutions for the community.

PART A: CONTEXT

A four-step process was used to develop this Plan:



Understand Current Conditions

The initial step set out to understand current conditions, experiences, and needs throughout Wayzata Boulevard. This phase focused on exploring existing conditions, such as existing and future land use, transportation safety and operations, physical conditions, economic data, and population growth forecasts, among other data to understand current conditions and the policies and actions that have informed these conditions.

Explore Opportunities and Vision

The second step built upon the understanding of current conditions to explore opportunities and priorities for Wayzata Boulevard from the community. This effort built from community needs and desires and technical analysis to craft a vision for the corridor and supplementary guiding principles.

Analyze Solutions/Solidify Recommendations

The third step of the process utilized the feedback and analysis of the first two steps to analyze and define draft strategies and recommendations to accomplish community desires. These draft strategies and recommendations were then reviewed with stakeholders to ensure that the final document would accurately reflect community understanding and support.

Implementation Strategies

The final step of the process analyzed the strategies and framework needed to implement the corridor vision. Actions to support future land use and development strategies, mobility improvements, and public realm considerations were identified and organized into the implementation plan.

PART A: CONTEXT

COMMUNITY ENGAGEMENT PROCESS

As a critical element of the process, an engagement plan was developed early in the process to outline the techniques and methods that would be used to collaborate with the community on this planning process. Engagement efforts were grouped into two primary phases:

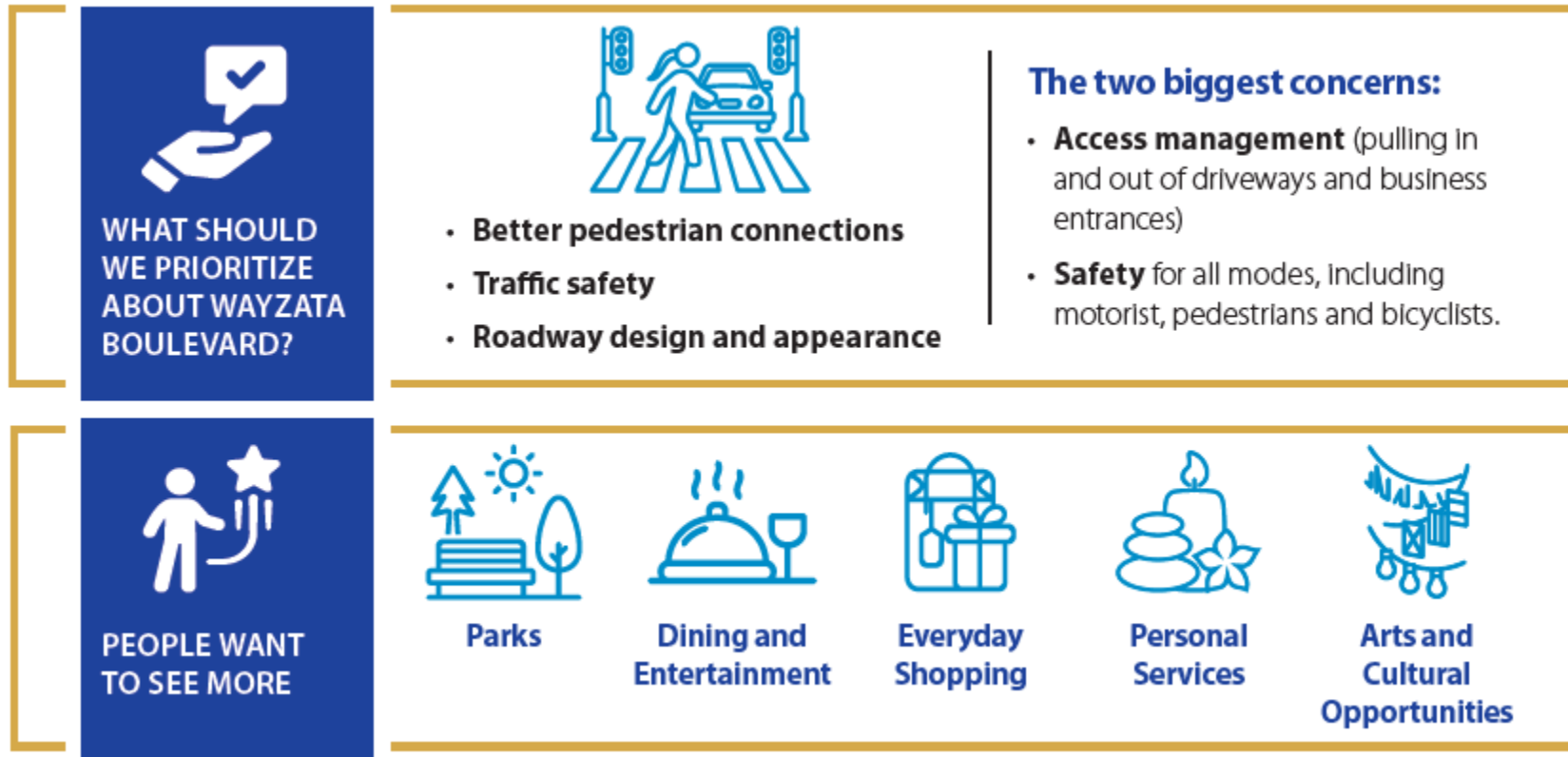
| Engagement Phase | Goal/Intent | Engagement Methods |
|------------------|---|--|
| Phase 1 | Initial data gathering and exploration phase to establish a corridor vision | <ul style="list-style-type: none">• Community Survey• Pop-Up Event• Focus Groups |
| Phase 2 | Explore opportunities and actions identified through a plan analysis. | <ul style="list-style-type: none">• Pop-Up Events• Open House• Stakeholder Conversations |



PART A: CONTEXT

Phase 1: Data Gathering and Exploration

The first phase of engagement included tailored engagement materials to learn about the community's existing experiences and opportunities for Wayzata Boulevard. Events included an online survey, a pop-up event at James J. Hill Days and a focus group with local businesses, property owners, and stakeholders. Through these events, the project team learned about the community's priorities and what they want to see more and less of.



PART A: CONTEXT

Phase 2: Explore Opportunities

The second phase of engagement provided an opportunity to engage with the community on potential strategies and solutions for Wayzata Boulevard. Events included pop-up events at Lunds & Byerlys and The Muni, an Open House, and Stakeholder Conversations. The input gathered through this phase provided an opportunity to ensure the study outcomes were aligned with the community's desires.



WHAT WERE
THE COMMON
THEMES?

- The development and built environment along Wayzata Boulevard create an opportunity to **maintain the charm and services** needed for the community. The boulevard should have its own identity.
- Infrastructure to support **movement by all modes** (e.g., walking, biking, and driving) is important, but **safety** for everyone must be prioritized.
- Enhancing the **plantings, landscaping and green spaces** along the corridor, along with **access to parks and recreation**, is desired.



WHERE ARE
INVESTMENTS
WANTED?



Additional
sidewalks
and trails



Improved
crossings
for bikes and
pedestrians



Increased
plantings and
landscaping



Investments
in parks and
open spaces



Improvements
to sight-lines

PART A: CONTEXT

HOW TO USE THE CORRIDOR STUDY

This Plan was designed to define a vision for Wayzata Boulevard based on community's preferences, including its visual and physical character, services, amenities, transportation system, future development, and experiences. It is intended to be used by city staff, developers, elected officials, economic development professionals, and regional stakeholders as a framework for future decision making. The Corridor Study should not be viewed as an uncompromising "blueprint" for what Wayzata Boulevard will look like, but rather a guide for future decisions to support the community's desires specific to the area.

WAYZATA BOULEVARD TODAY

In its current state, the corridor provides a traffic thoroughfare that supports local and regional trips. The current character of the eastern and western segments of Wayzata Boulevard is a legacy of its former state highway designation. However, the vision established by the 2040 Comprehensive Plan demands a reconsideration of the physical configuration and potential for future improvement. In addition to Wayzata Boulevard, the study incorporates a functionally similar section of Central Ave N from Wayzata Blvd E to Highway 12. The corridor plan was highlighted as an important project in the City's Strategic Plan and 2040 Comprehensive Plan.

Wayzata Boulevard is a former state highway, and it has been noted that there are concerns about the number and severity of the crashes in the corridor, specifically (East/West) of, and including Central Avenue. There are also gaps in the pedestrian and bicycle networks and the local transit line is infrequent and underutilized.

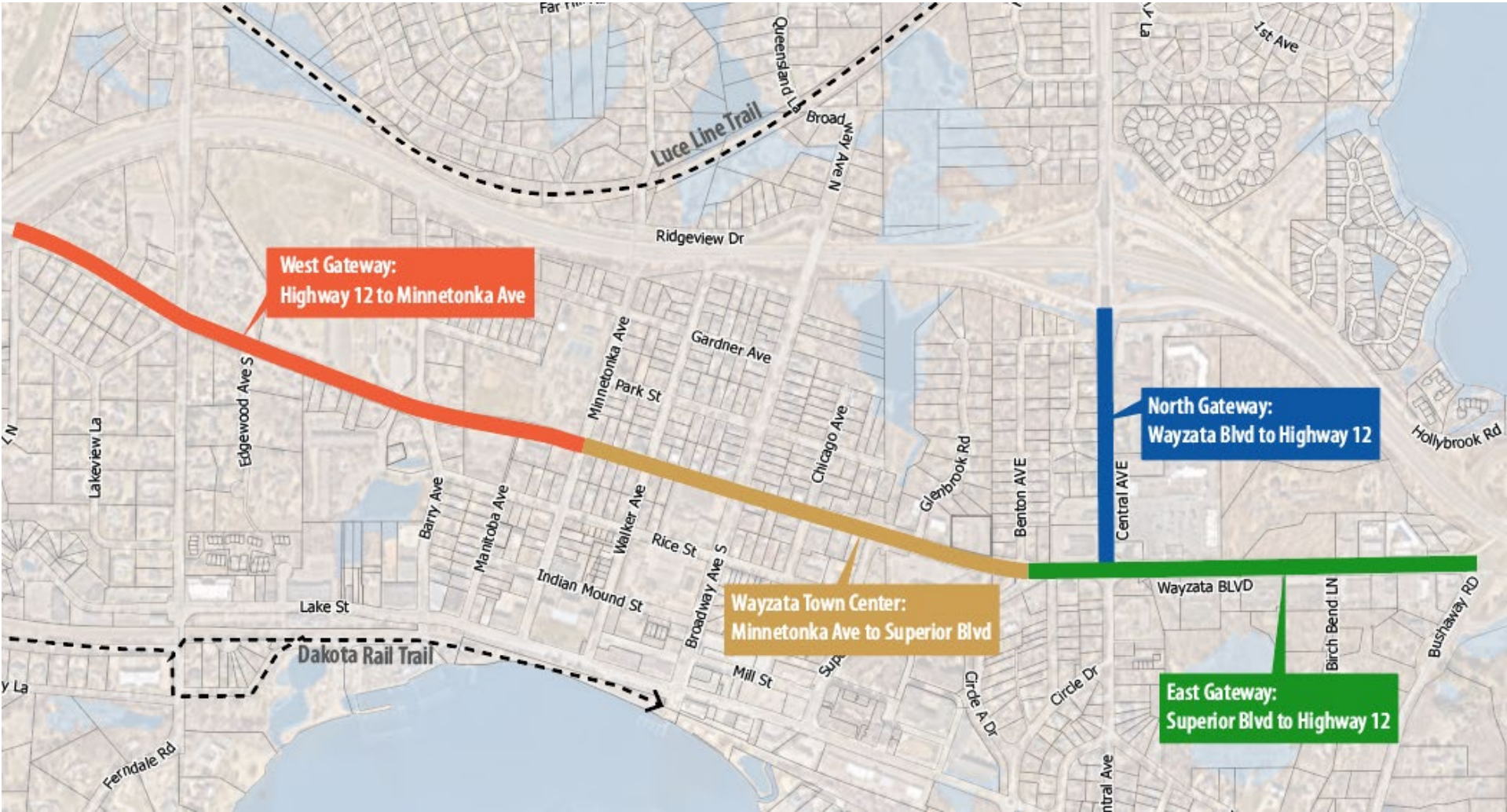


Wayzata Boulevard looking west from the Big Woods Preserve

PART A: CONTEXT

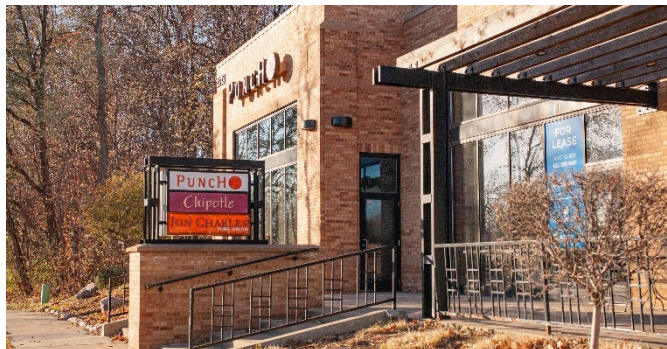
STUDY AREA

Wayzata Boulevard stretches from the eastern to western edge of the community. The study area is broken down into four individual areas, each with varying characteristics. The map and study area descriptions below provide an initial understanding of each gateway segment.



PART A: CONTEXT

East Gateway: Hwy 12 to Superior Boulevard



Punch Pizza and Chipotle looking west

The East Gateway segment is the main business area along Wayzata Boulevard and a Hennepin County roadway. This auto-focused area includes a variety of commercial businesses, including drive-thru restaurants, retail businesses, and office locations. The roadway includes a 5-lane section, including two travel lanes in each direction with a center left-turn lane. A sidewalk is available on either side of the roadway for many parts of this segment.

North Gateway: Central Avenue from Wayzata Boulevard to Hwy 12

The North Gateway is the only segment outside of the Wayzata Blvd corridor. This segment is included in the study because it functions similar to Wayzata Boulevard east of Superior Blvd as a busy conduit connecting to Highway 12. It has a strong commercial presence and includes single-family homes on the west side. Central Avenue is also under Hennepin County's jurisdiction and includes two travel lanes in each direction.

Town Core: Minnetonka Avenue to Superior Boulevard

The Town Core segment has a mixture of land uses in the central core neighborhood with commercial, multi-family, school, and single-family homes fronting on a road that underwent a two-lane conversion and landscaped medians in the mid-2010's. This conversion created a two-lane street with a landscape median and sidewalks on either side.

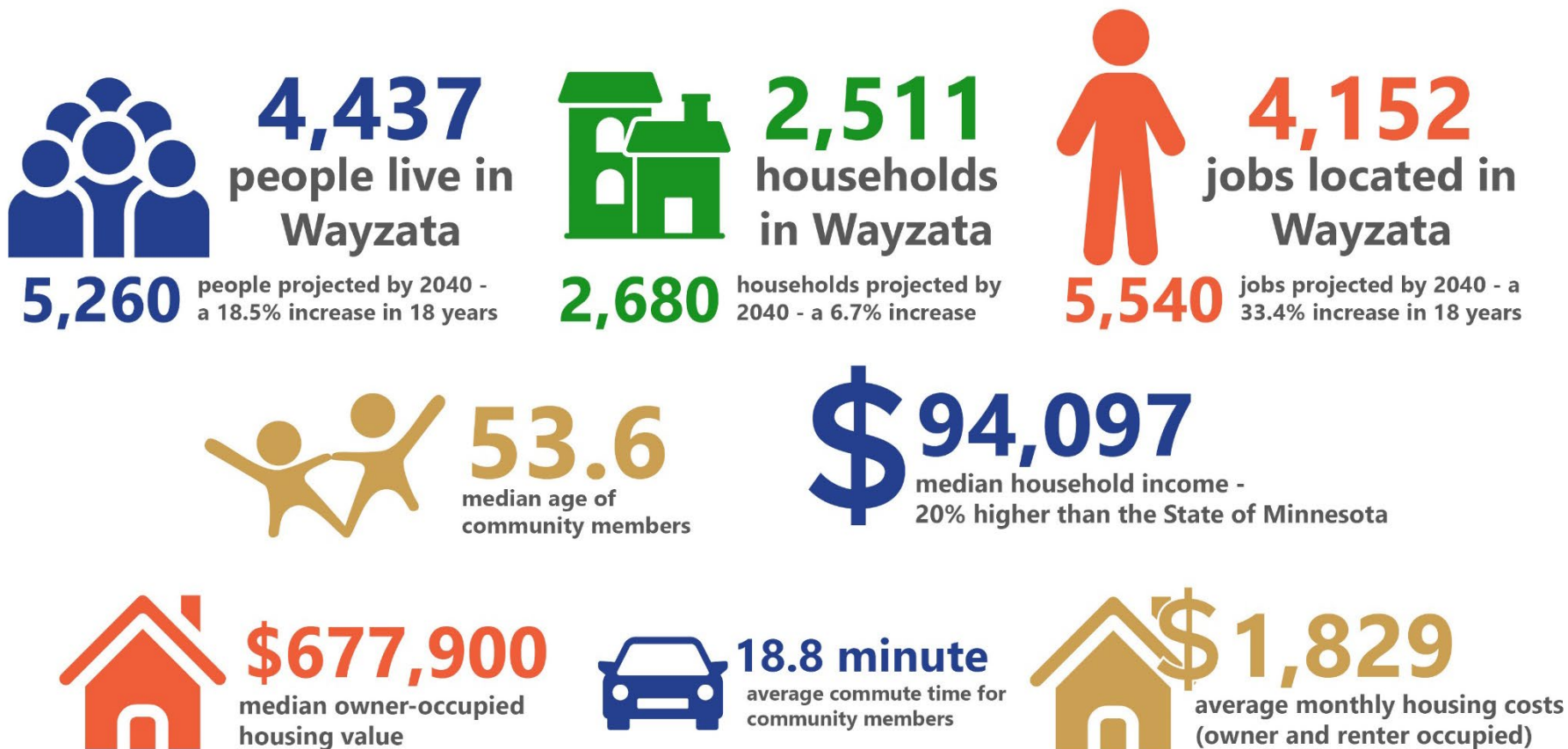
West Gateway: Hwy 12 to Minnetonka Avenue

The West Gateway segment has larger parcels of land and more institutional ownership than the other segments as well as more single-family homes, multi-family, recreational facilities, and a current park and ride site. This section continues the two-lane roadway without the center median. Sidewalks are generally consistent with some gaps on either side; however, the distance from the curb line varies. There will likely be less development pressure along this segment due to existing single-family home developments and large public uses. This area may provide future regional trail connectivity between the Luce Line and Dakota Rail trails, so balancing pedestrian and cyclist safety and connections with vehicular traffic are significant priorities.

PART A: CONTEXT

SOCIAL CONTEXT

Exploring the varied characteristics of the community is one way to explore potential issues and opportunities that apply today and into the future. The social context of the community was explored in the early phases of this effort to provide an understanding of the community. Key points are highlighted in the graphic below. The full social context analysis can be found in [Appendix B](#).



PART A: CONTEXT

DEVELOPMENT AND LAND USE CONTEXT

Existing Land Use

Existing land use categories identify various residential, commercial, public, and open space uses throughout the community. Within the study area, the primary uses adjacent to the corridor include Commercial, Central Core Residential, High Density Residential, Parks, and Institutional/Public as described below.



Existing Mixed Use in the Town Core

| Land Use Category | Description |
|---------------------------------|---|
| Central Core Residential | The Central Core Residential category represents the single-family residential development located in the central core of Wayzata. These homes include smaller lot sizes, with densities of 3 to 6 units per acre. Single-family units are the primary uses within this district. |
| High Density Residential | The High-Density Residential category allows for the highest density standalone residential uses at a density of 12 to 30 units per acre. Residential uses within this category predominately include multi-unit and multi-story buildings. |
| Commercial | The Commercial category represents traditional retail sales and service uses. This broad category includes office uses, shopping opportunities, and other general service uses. |
| Public/Institutional | The Institutional/Public category represents publicly owned buildings and properties that typically provide a community service. This can include schools, churches and government buildings. |
| Parks | The Parks category represents recreation facilities and land owned by the city or other public jurisdictions. These uses provide some form of recreation or public use for the general public. Park uses can include a dedicated city park, sports field or nature preserve. |

PART A: CONTEXT

Future Land Use

The 2040 Comprehensive Plan serves as the community’s guiding document for development decisions, with a key tool being the future land use plan. Many of the existing land use categories are consistent with the future land use plan, other than the designation of a new land use category, Mixed-Use Commercial/Residential. This category was created as part of the 2040 Comprehensive Plan to create opportunities for redevelopment within the community that supported both the existing commercial development with opportunities for residential growth.

| Land Use Category | Description |
|---|--|
| Mixed-Use Commercial/Residential | The Mixed-Use Commercial/Residential land use category represents a broad range of uses that represent retail, service, office and multi-family residential. |

The intent of the Mixed-Use Commercial/Residential category is to encourage development that responds to the characteristics and locality of the property, while encouraging a mixture of development types to serve residents. The category has been designated in areas that are traditionally commercial uses. This designation allows for the continued use of commercial development but encourages the inclusion of residential uses. Areas designated for the Mixed-Use Commercial/Residential category are primarily located along the Wayzata Boulevard corridor.

Naturally Occurring Affordable Housing

Housing affordability is measured by the ratio of housing costs to income. Generally, affordable housing should cost no more than 30 percent of a household’s annual income. For example, a household earning \$100,000 and spending \$30,000 or less annually would be considered affordable. This level of affordability varies from community to community. For example, Wayzata residents had a median household income of \$94,097 in 2020, which would equate to a median affordable housing cost of \$2,352 per month.

The Metropolitan Council has defined regional affordable housing guidelines and policies that inform Naturally Occurring Affordable Housing (NOAH) calculations and program eligibility. The regional affordable housing policy measures affordability limits for rental units separately from owner-occupied units. For 2023, the rental affordability limit is 60 percent of the area median income and 80 percent of area median income for ownership affordability. For the region, the affordable rental cost for a 2-bedroom unit is \$1,398 per month (including utilities and the affordable home price is \$304,700.

Additionally, the 2040 Comprehensive Plan identifies the regional goals for the development of additional affordable housing. Maintaining existing affordable housing units within the community is one tool for providing affordability. There are several Naturally Occurring

PART A: CONTEXT

Affordable Housing (NOAH) uses along the corridor. NOAH properties have been identified on the illustrative plans in each section. As development is proposed along the corridor, the preservation or enhancement of these NOAH properties should be considered.

MOBILITY CONTEXT

Understanding how people move throughout a community and study area provides insight into overall patterns and trends. Whether by car, bike, foot, chair, or bus, all modes of transportation should be explored, understanding the infrastructure dedicated to each mode and the resulting access, connectivity, and safety.

Roadways

The study area for the Wayzata Boundary Corridor Study encompasses all of Wayzata Boulevard from Highway 12 on the west side to Highway 12 and Bushaway Road on the east side and Central Avenue from Wayzata Boulevard to Highway 12. There are 15 intersecting roadways throughout the study area including two county highways, 12 city streets, and 1 US Highway. The City of Wayzata and Hennepin County both have jurisdiction over roadways within the study area. Hennepin County has jurisdiction of Highway 101, also known as Central Avenue from Highway 12 to Wayzata Boulevard and Wayzata Boulevard from Central Avenue to Highway 12. The Minnesota Department of Transportation maintains jurisdiction of US Highway 12. All other roadways are local roadways under the jurisdiction of the City of Wayzata.

Roadway Volumes

Existing and forecasted traffic volumes provide an understanding of the overall use of a roadway and can be used to analyze potential cross sections and design improvements. The most recent traffic volumes collected within the study area were collected as part of the Comprehensive Plan. Volumes are presented as the number of trips on that segment of roadway on a daily basis. The existing volumes identified within the Comprehensive Plan range from 10,000 to 21,800 vehicles per day (vpd) throughout the study area. The Comprehensive Plan also forecasted future traffic volumes for the year 2040, based on development trends and anticipated growth rates. This analysis



Wayzata Boulevard and Minnetonka Avenue

PART A: CONTEXT

forecasted growth to 12,300 to 25,000 vehicles per day within the study area. The following table identifies the existing and forecasted volumes for each segment of the corridor.

| Roadway Segment | Existing Volume | 2040 Forecasted Volume | % Increase |
|--|-----------------|------------------------|------------|
| Wayzata Boulevard – from Highway 12 to Minnetonka Avenue | 10,000 vpd | 12,300 vpd | 23.0% |
| Wayzata Boulevard from Minnetonka Avenue to Superior Boulevard | 14,700 vpd | 17,000 vpd | 15.6% |
| Wayzata Boulevard from Superior Boulevard to Highway 12 | 21,800 vpd | 25,000 vpd | 14.7% |
| Central Avenue from Wayzata Boulevard to Highway 12 | 17,400 vpd | 22,000 vpd | 26.4% |

Source: 2040 Comprehensive Plan, MnDOT



Wayzata Boulevard and Ferndale Road Intersection

Traffic volumes along Wayzata Boulevard have remained relatively consistent over the past decade, with increased projected over the next 20 years (accounting for both new development within the study area and natural increases), which must be considered as roadway changes are explored. The capacity of a roadway is a measure of congestion that is influenced by the number of lanes, access management, and transit service offerings. In addition to exploring the current capacity of the roadways, comparison to other regional roadways was completed to compare traffic volumes, roadway design, and capacity to understand experiences. [Appendix D](#) highlights the roadways used for comparison.

PART A: CONTEXT

Bike and Pedestrian Infrastructure

In addition to the roadways within the study area, bicycle and pedestrian infrastructure rounds out the mobility options along Wayzata Boulevard. Bicycle and pedestrian infrastructure include both the dedicated travel facilities (i.e., sidewalks, trails, and lanes) and the facilities or improvements that aid movement by walking, biking, and rolling (i.e., crosswalks, median refuge, and pedestrian signs/lights). Infrastructure within the study area primarily includes sidewalks that range in width from 4 to 8 feet along with painted and/or signed crosswalks. Recent improvements at some intersections include median refuge opportunities, allowing pedestrians to cross halfway before the next break in traffic. There are a number of gaps in the sidewalk network that inhibit connectivity, particularly on the south side of Wayzata Boulevard. These identified gaps include gaps where no specific accessible pedestrian facility is provided.

Dedicated bicycle infrastructure is not present along Wayzata Boulevard. By law, bicycles have the right of way to share a driving lane with vehicles when no other facility is available. Additionally, traditional 4- to 6-foot sidewalks are designed for pedestrian traffic or the walking of bicycles. Dedicated on-street bike infrastructure is available adjacent to the corridor on Barry Avenue and Ferndale Road. These treatments include bike lanes and sharrows, respectively.

There are a number of intersections with infrastructure to support the safe crossing of bicycles and pedestrians within the corridor. Many of the existing intersections have paved crosswalks to indicate a crossing location to all modes. Additionally, intersections controlled by a traffic signal provide a pedestrian controlled crossing and timing system to facilitate crossing.

Crash History

Crash data from the Minnesota Crash Mapping Analysis Tool (MnCMAT) was reviewed for the years 2012 through 2021. Five years of historic crash data is reviewed for all transportation modes. Ten years of historic crash data was reviewed for crashes involving pedestrians and bicyclists. Within the years 2017 through 2021, 258 crashes were reported on Wayzata Blvd and Central Ave within the study area. Approximately 63 percent of the total crashes were Rear End and Angle crash types. For the analysis of the corridor study, we are particularly interested in crashes that are a result of roadway design or function, rather than weather or other related incidents. Approximately 28 percent of total crashes involved Serious or Minor Injuries. The majority of crashes, 71 percent, were classified as Property Damage Only, indicating the no injury occurred. Note that of the crashes reviewed, one was fatal (2017), and one involved serious injury (2021).



Existing Flashing Beacon at Barry Ave

PART A: CONTEXT

Crash Types, 2017 - 2021

| Crash Type | Total Crashes | % of Total |
|------------------------------|---------------|------------|
| Pedestrian | 4 | 1.6% |
| Bike | 1 | 0.4% |
| Single Vehicle Run Off Road | 16 | 6.2% |
| Single Vehicle Other | 2 | 0.8% |
| Sideswipe Same Direction | 30 | 11.6% |
| Sideswipe Opposing Direction | 4 | 1.6% |
| Rear End | 66 | 25.6% |
| Head On | 4 | 1.6% |
| Left Turn | 22 | 8.5% |
| Angle | 98 | 38.0% |
| Other | 11 | 4.3% |
| Total | 258 | -- |

Crash Severity, 2017 - 2021

| Crash Type | Total Crashes | % of Total |
|----------------------|---------------|------------|
| Fatal | 1 | 0.4% |
| Serious Injury | 1 | 0.4% |
| Minor Injury | 33 | 12.8% |
| Possible Injury | 40 | 15.5% |
| Property Damage Only | 1883 | 70.9% |
| Total | 1,958 | -- |

The majority of crashes were localized on the segment of Wayzata Blvd between Central Ave and Highway 12 to the east, and on the segment of Central Avenue between Wayzata Blvd and Highway 12 to the north. These areas not only have higher daily traffic volumes than the western segment; they also have access in closer proximity, and more complex lane configurations for travelers to navigate. Exploring the crash types and their density along various segments of the roadway is helpful for understanding opportunities for improvements. For example, there is a high density of Angle Crashes along the eastern segment of Wayzata Boulevard where there is also a higher density of local access points. Additionally, there is a density of rear end crashes (also identified as front to rear) along Wayzata Boulevard between Superior Boulevard and Central Avenue. This area includes two traffic signals and is an area of many turning movements off of the corridor. At least 12 of the 66 rear end crashes are located in this short segment of the corridor.

Other factors of the crash history should also be examined to understand a complete picture of any safety problems or considerations. As a winter season community, environmental factors are also a contributing factor to crashes. While some crashes are inevitable due to driver activity, the conditions of the roadway (e.g., snow covered, wet) and the season can influence crashes or increase severity.

PART A: CONTEXT

Pedestrian and Bicycle Crashes

Pedestrian and bicycle crash data from the Minnesota Crash Mapping Analysis Tool (MnCMAT) was reviewed for the years 2012 through 2021. Within the years 2012 through 2021, 12 of the total 508 crashes reported on Wayzata Blvd and Central Ave involved a pedestrian or bicyclist. Nine crashes involved pedestrians and three involved bicyclists: of these, two involved serious injuries.

The majority of pedestrian and bicycle crashes followed the larger all-traffic crash trends of occurring on the segment of Wayzata Blvd between Central Ave and US 12 to the east, and on the segment of Central Avenue between Wayzata Blvd and US 12 to the north.



Wayzata Blvd at Central Ave looking west

Pedestrian and Bicycle Crashes, 2012 - 2021

| Crash Year | Crash Type | Severity |
|------------|------------|-----------------|
| 2012 | Pedestrian | Minor Injury |
| 2013 | Bike | Minor Injury |
| 2014 | Pedestrian | Possible Injury |
| 2015 | Pedestrian | Possible Injury |
| 2015 | Bike | Serious Injury |
| 2016 | Pedestrian | Serious Injury |
| 2016 | Pedestrian | Minor Injury |
| 2017 | Pedestrian | Minor Injury |
| 2017 | Pedestrian | Minor Injury |
| 2019 | Pedestrian | Minor Injury |
| 2019 | Bike | Possible Injury |
| 2020 | Pedestrian | Possible Injury |

PART B: VISION & GUIDING PRINCIPLES

PART B: VISION & GUIDING PRINCIPLES

Wayzata Boulevard is a key feature of the Wayzata Community. The corridor has many functions, each evolving with development changes, public investments, and economic growth. On average, over 30,000 vehicles use at least one of the three study area termini to enter or exit the community each day. The transportation infrastructure and adjacent development inform the experiences and impressions for each of these travelers.

WHAT IS A CORRIDOR VISION?

The Wayzata Boulevard Corridor Study is intended to define policy and implementation actions to support development, mobility, and public realm investments for years to come. A corridor vision provides a framework for supporting future decisions, public investments, and partnerships that support long-term community plans and needs. The vision statement needs to be broad enough to provide long-term guidance for the entire corridor, but with enough specificity to represent community input and guide investment.

Guiding Principles are an additional tool to support decision making and the corridor vision. They are intended to provide a big-picture direction and priorities for the corridor. They represent the community's needs and desires and connect to future investment and policy decisions. These elements will specifically inform the recommendations of the plan.



Existing Mixed-Use in the Town Core

PART B: VISION & GUIDING PRINCIPLES

COMMUNITY INPUT

Community input was a cornerstone of the Wayzata Boulevard Corridor Study process. Multiple themes were identified through engagement opportunities and these themes informed the development of the corridor vision and supporting guiding principles.

- **Safety:** Safety for all modes should be prioritized through moderation of speed and street design.
- **Manage Mobility:** A balance between local access and overall mobility is desired to meet the various user needs.
- **Parks and Open Space:** The existing parks and open spaces in the study area are highly valued and should be prioritized for investment and maintenance.
- **Arts and Culture:** The corridor provides an opportunity to showcase the community's culture and charm.
- **Design and Appearance:** Development trends and patterns have shaped the corridor today and recent zoning and regulation updates are shaping the corridor appearance.
- **Development:** There are many valued and established developments along the corridor and a number of opportunities for investment and change. Wayzata Boulevard includes day to day retail and service commercial uses that are only available within the community along the corridor. Development opportunities along the corridor can take many forms, including a combination of both public and private investment.
- **Identity:** Wayzata Boulevard supports the overall charm and identity of the community and serves as a key gateway to greet visitors and welcome residents home.
- **Multimodal:** Wayzata Boulevard should support safe and efficient movement by all modes. Where provided, multi-modal improvements will be context-appropriate and implemented to reinforce existing networks.
- **Goods and Services:** Development along the corridor provides essential goods and services for residents, and access to these destinations is important.
- **Residential:** Residential development has been a component of the corridor for many years, and future growth can take many forms, but will remain compatible with Wayzata today.

PART B: VISION & GUIDING PRINCIPLES

CORRIDOR VISION

The vision for Wayzata Boulevard embraces the community's vision and mission:

Wayzata Vision:

Wayzata is a charming, socially connected, and walkable lakeside community. A multi-generational community at the forefront of sustainability, with a healthy environment, enticing parks and public spaces. It is a proud steward of its premier natural asset; Lake Minnetonka.

Wayzata Mission:

To preserve and enhance the quality of life of the community, in order to be the idyllic city on Lake Minnetonka to live, work, shop and play.

WAYZATA BOULEVARD VISION

Wayzata Boulevard is a central element of the community's identity, providing connections, spaces, and destinations for community members and visitors. The corridor provides an opportunity for social, physical, and employment connections. The Wayzata Boulevard vision embraces the following key elements:

SENSE OF PLACE AND IDENTITY: Wayzata Boulevard serves as the gateway to the community, greeting visitors and welcoming residents home, so it is imperative to create unique opportunities to highlight the community charm and identity.

MOBILITY OPTIONS: Investment in Wayzata Boulevard will support safe and efficient mobility for all transportation modes.

PEDESTRIAN SAFETY: While safe transportation for all is important, investment to improve and complete the pedestrian network should be a priority.

RESIDENTIAL VIBRANCY: Wayzata Boulevard and the surrounding area is the heart of the Wayzata community. Fine grained land use adjustments and overlay zoning will provide opportunities to enhance existing residential areas and allow for varied redevelopment.

DESTINATIONS AND ACCESS: Many destinations within the community can be accessed from multiple modes. Balancing needed access with overall mobility goals is a priority.

RANGE OF EMPLOYMENT: Wayzata Boulevard businesses provide a range of employment opportunities within the community, create destinations and services available to community members, provide needed community services, and welcome visitors into the community.

PART B: VISION & GUIDING PRINCIPLES

GUIDING PRINCIPLES

To support the Wayzata Boulevard corridor vision, eight guiding principles were created to support the specific direction and guidance to achieve the vision. The principles should be used as a tool for evaluating and directing future plans, decisions and investments. They encompass the corridor's future identity, safety, and public realm investments.

As future public and private investments are explored, the guiding principles should be used as a primary tool for evaluating projects and their fit within the plan. They are also intended to be used as a tool and framework for future policies, plans, and decision making.

1

Wayzata Boulevard serves as the **GATEWAY TO THE COMMUNITY**, creating opportunities to showcase and introduce community charm and identity through design, public infrastructure, and guided development.

2

Provide **SAFE, CONVENIENT**, and **PLEASANT** transportation options for all, through user-friendly features, such as sidewalks, crosswalks, bike lanes, lighting, signage, and more.

3

Expand on the diverse uses and development types of the corridor to optimize **ECONOMIC DEVELOPMENT** and **HOUSING** growth and investment opportunities.

4

Leverage the regional goals and opportunities to **INCREASE HOUSING ACCESS** and **AFFORDABILITY** through new housing opportunities along the corridor.

PART B: VISION & GUIDING PRINCIPLES

5

Promote investment, enhancement, and stewardship of the **NATURAL AND OPEN SPACES** along the corridor, expanding opportunities to enhance access for all.

6

Capture the value of existing parks and recreational destinations by exploring **PHYSICAL CONNECTIONS** and leverage the resources available to the community.

7

Promote **INVESTMENT** and **REDEVELOPMENT** along the Wayzata Boulevard Corridor to support the community's goals and needs for **EMPLOYMENT AND SERVICE OFFERINGS**.

8

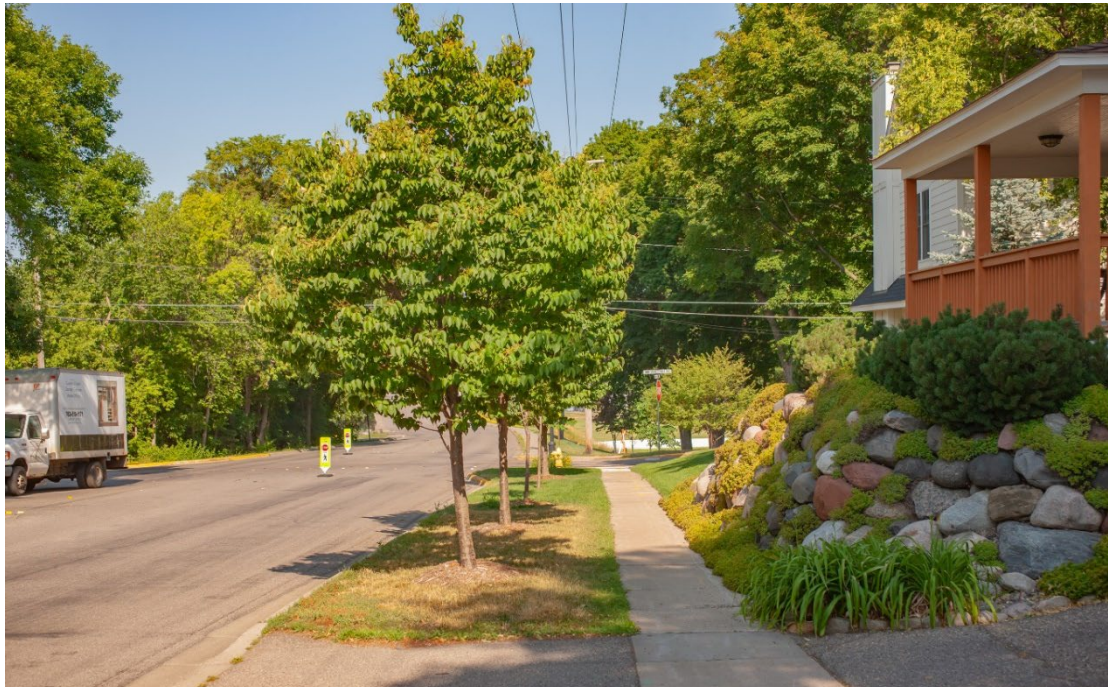
Create a **UNIFIED CORRIDOR** that builds on the overall identity of Wayzata, promotes its future potential, celebrates public art, and provides its own **UNIQUE SENSE OF PLACE**.

PART C: FRAMEWORK & CORRIDOR STRATEGIES

INTRODUCTION

Corridor strategies were developed and analyzed to lay the framework for future implementation and decisions making that supports the community's vision for the corridor. This framework is organized into five sections – Framework Overview and the Segment Plans.

- **Wayzata Boulevard Framework Overview:** This section reviews the concepts and considerations that apply to the entirety of the study area and identifies the general framework for strategies.
- **Wayzata Boulevard Segment Specific Plans:** These four plans tailor the overall strategies to the specific segment of the study area to create context specific solutions. Illustrative plans are provided for each gateway plan to illustrate the recommendations across the four focus areas.



Town Core looking west

PART C: FRAMEWORK & CORRIDOR STRATEGIES

WAYZATA BOULEVARD FRAMEWORK OVERVIEW

The framework for each topical area (land use and development; safety improvements; transportation and mobility; and public realm improvements) establishes an overall approach that applies to the entirety of the study area. This framework provided the opportunity to create strategies and recommendations specific to each of the study area segments. Details of the overarching approach to each area should be reviewed to support the specific strategies described in **Part D: Implementation**.

LAND USE AND DEVELOPMENT



What is Land Use and Development?

Land Use and Development supports overall guidance for land use decision making and strategies for development and redevelopment investments within the community.

Common Engagement Themes

Service uses: The Wayzata Boulevard corridor supports some of the only service-based uses in the community, and access to those uses should be maintained.

Access to housing: Access to a variety of housing types that are affordable and available for new or transitioning community members is desired.

Connecting community spaces: While there is a wide range of use types along the corridor, the community emphasized the connection between uses and the desire for access and connection, particularly to green spaces.

Land Use and Development Framework Elements

Land use districts
Specific land use opportunities
Physical connection

The Wayzata Corridor Study is intended to establish an overall vision for the corridor with specific land use, transportation, safety, and streetscape strategies to achieve that vision. The land use and development component of the study is intended to build from the recommendations and guidance of the city's 2040 Comprehensive Plan and serve as a small area plan with additional guidance and recommendations beyond the traditional future land use approach. Recommendations identified within this section should be viewed as flexible, allowing the plan to respond to changing trends and needs through the decision-making process. This guidance is not intended to be used as a proactive redevelopment plan; rather, it is a framework of strategies for implementation when the timing is appropriate.

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While the Comprehensive Plan will continue to serve as the primary guidance document for development decisions within the community, the Wayzata Boulevard Corridor Study identifies additional guidance, strategies and solutions that can inform future investments and decision making. In order to best serve as a supplement to the Comprehensive Plan, the land use opportunities along the corridor should be built from the future land uses identified in the 2040 Plan.

As noted, there are five land use categories used to guide development within the study area. These five categories provide broad context and recommendations to inform zoning and development decisions. However, analysis and community input within the study found that further refinement of the mixed-use category to align with current and future uses was warranted. For example, mixed-use development opportunities for a large parcel, like the Colonial Square site at 1125 Wayzata Boulevard East, have different potential than smaller parcels, like the former gas station at 1022 Wayzata Boulevard East to the south.









The land use framework within this Plan provides additional recommendations that can be used to inform future zoning updates that support the goals of this plan and the 2040 Comprehensive Plan when guiding mixed use development. The Comprehensive Plan will continue to serve as the primary document for informing land use decisions, and this Plan should support those decisions with the additional guidance and recommendations identified. The refined land use districts are identified in the following figure.



Bicyclist headed east on Wayzata Boulevard

Wayzata Blvd



- | | |
|---|--|
|  Auto Oriented Commercial |  Neighborhood Mixed-Use |
|  Office, Service, and Mixed-Use |  Parks and Open Spaces |
|  High Intensity Mixed-Use |  Public Spaces |
|  Medium Intensity Mixed-Use |  Neighborhood Residential |



PART C: FRAMEWORK & CORRIDOR STRATEGIES

Mixed-Use Districts

The Mixed-Use Commercial/Residential category allows for a range of uses that represent retail, service, office, and multi-family residential. This broad category can take many forms, including vertical mixed-use with ground floor commercial and upper floor residential or horizontal mixed-use with residential uses adjacent to commercial uses. There are a number of opportunities for mixed-use within the study area that have been organized into the following districts:

- Auto Oriented Commercial
- Office, Service and Mixed-Use
- High Intensity Mixed-Use
- Medium Intensity Mixed-Use
- Neighborhood Mixed-Use

Auto Oriented Commercial

The Auto Oriented Commercial District is intended to support general commercial uses that are primarily oriented to access by vehicles. Examples include gas stations, food and beverage drive-thrus, and other service-oriented uses.

| | |
|------------------------------|---|
| Guided Uses: | General commercial uses focused on auto oriented uses. This district would be the only one to allow food and beverage drive-thrus as a conditional use, along with other auto-centric uses (e.g., gas station). |
| Building Height: | Maximum three-story |
| Building Orientation: | Structures should be oriented on the site for the proper management of circulation within the site for vehicles and relationships to access Wayzata Boulevard. |
| Residential Density: | N/A |
| Parking and Access: | Future development to be guided for shared access and parking as much as possible. Careful consideration of queuing for drive-thrus and site circulation is needed. |
| Other Considerations: | This district would be the only area where drive-thrus would be considered. |

PART C: FRAMEWORK & CORRIDOR STRATEGIES

Office, Service and Mixed-Use

The Office, Service and Mixed-Use District is intended to support general commercial uses with opportunities for residential uses. This district provides opportunities for residential development, but it is not a primary focus.

| | |
|------------------------------|--|
| Guided Uses: | General commercial uses with limited opportunities for vertical mixed-use (residential on upper floors). Ground floor uses may include any arrangement of public and commercial uses. Examples may include a ground floor retail store or a community room managed by the residential manager. |
| Building Height: | Maximum three stories, with stepbacks required at the third story as identified by the <i>Wayzata Design Standards</i> . |
| Building Orientation: | Structures should be oriented to the street with parking located in the rear |
| Residential Density: | 10 to 30 units/acre as allowed within the Comprehensive Plan, with opportunities for density bonuses as allowed within the city's code. |
| Parking and Access: | Future development should include opportunities for shared access to adjacent lots with parking located in the rear. Access points should be avoided within 100 feet of an intersection and should be spaced at least 300 feet from another access. |
| Other Considerations: | The district supports coordination between existing uses and future development opportunities. |

High Intensity Mixed-Use

The High Intensity Mixed-Use District is intended to support the highest density mixed-use development within the community. The district supports a combination of vertical and horizontal mixed-use. Additional development may support service or office uses on upper floors, in addition to residential uses.

| | |
|-------------------------|--|
| Guided Uses: | General commercial and high-density residential uses as horizontal or vertical mixed-use developments. Ground floor uses may include any arrangement of public and commercial uses. Examples may include a ground floor retail store or a community room managed by the residential manager. |
| Building Height: | Maximum four stories, with a defined amount of affordable housing units included within the development. Additionally, stepbacks required for the fourth story as identified within the <i>Wayzata Design Standards</i> . |

PART C: FRAMEWORK & CORRIDOR STRATEGIES

| | |
|------------------------------|---|
| Building Orientation: | Structures should be oriented to the street with parking located in the rear. |
| Residential Density: | 10 to 30 units/acre as allowed within the Comprehensive Plan, with opportunities for density bonuses as allowed within the city's code. |
| Parking and Access: | Future developments are to be guided for shared access and parking as possible with parking located in the rear. Access points should be avoided within 100 feet of an intersection and should be spaced at least 300 feet from another access. Tools to manage parking uses throughout the day can be used to limit the overall parking need (day versus evening parking). |
| Other Considerations: | This district provides opportunities to increase access to affordable housing through the density bonus tool to provide meaningful density increases throughout the corridor. |

Medium Intensity Mixed-Use

The Medium Intensity Mixed-Use District is intended to support similar uses to the High Intensity Mixed-Use District on a smaller scale within the community. The district supports a combination of vertical and horizontal mixed-use. Additional development may support service or office uses on upper floors, in addition to residential uses.

| | |
|------------------------------|---|
| Guided Uses: | General commercial and high-density residential uses as horizontal or vertical mixed-use developments |
| Building Height: | Maximum three stories, with stepbacks required at the third story as identified by the <i>Wayzata Design Standards</i> . |
| Building Orientation: | Structures should be oriented to the street with parking located in the rear. |
| Residential Density: | 10 to 30 units/acre as allowed within the Comprehensive Plan, with opportunities for density bonuses as allowed within the city's code. |
| Parking and Access: | Future development to be guided for shared access and parking as possible with parking located in the rear. Additionally, tools to manage parking use throughout the day can be used to limit the overall parking need (day versus evening parking). Access points should be avoided within 100 feet of an intersection and should be spaced at least 300 feet from another access. |
| Other Considerations: | This district provides opportunities to increase access to affordable housing with higher densities. Additionally, horizontal mixed-use may function best within this district and create additional housing options. |

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Neighborhood Mixed-Use

The Neighborhood District is intended to support existing single-family and multi-family residential uses with the introduction of small-scale commercial uses. These uses exist today – from offices to in-home occupations. Generally, the district is intended to support the continued residential uses with possibilities for neighborhood commercial to be considered on smaller lots along the corridor.

| | |
|------------------------------|---|
| Guided Uses: | Primarily single-family and multi-family residential uses with opportunities for neighborhood commercial uses, including personal services, professional services, and limited retail. The C-1A and C-1B zoning districts outline a framework that can be used to execute the mixed of uses within this district. |
| Building Height: | Maximum three stories, with stepbacks required at the third story as identified by the <i>Wayzata Design Standards</i> . |
| Building Orientation: | Development should be guided to align with existing development forms in the neighborhoods. Commercial uses should respect adjacent residential uses and fit within the context. For example, a consistent setback across the entire district provides a similar look and feel for travelers as they move along the corridor. |
| Residential Density: | 3 to 6 units per acre, but up to 30 units per acre for lots with existing multi-family residential. Bonus densities may also be available as allowed within the zoning ordinance. |
| Parking and Access: | Individual access to be provided from side streets with parking requirements to align with the proposed use. |
| Other Considerations: | This district promotes the maintenance and enhancement of the existing neighborhoods with additional opportunity for services. |

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Public Districts

The Public Districts represent uses and spaces that provide a public asset or service within the community. The three land uses that align with these districts are the Institutional/Public, Parks, and Public Open Space land uses. These uses each align with open space and recreation areas or public services/resources (e.g., schools, religious institutions). For the purposes of this study, it is assumed that the following district will generally remain as they exist today:

- Parks and Open Space
- Public Spaces

Parks and Open Space

The Parks and Open Space District is intended to identify existing recreational and natural resources within the study area. These are fixtures within the community and contribute to the quality of life and charm of the community. Stewardship and enhancement of these uses are supported within the plans’ recommendations.

| | |
|------------------------------|--|
| Guided Uses: | Open space, parks, and outdoor recreation |
| Building Height: | One story |
| Building Orientation: | Amenities and structures should be oriented to best align with the specific uses on site. |
| Residential Density: | N/A |
| Parking and Access: | If parking is provided, proper access to be provided from a side street to a small lot. Access from trails and sidewalks is highly encouraged. |
| Other Considerations: | Stewardship of existing resources and further enhancement is a major goal within this district. |

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Public Spaces

The Public Spaces District is intended to support existing institutional or public uses within the study area. From schools to religious institutions, there are a number of these uses along the corridor that contribute to the sense of place and identity of the corridor. Generally, these uses are supported to remain within the study area.

| | |
|------------------------------|--|
| Guided Uses: | Public or Semi-Public Uses |
| Building Height: | Maximum three stories, with stepbacks required at the third story as identified by the <i>Wayzata Design Standards</i> . |
| Building Orientation: | Development should be guided to align with existing development forms in surrounding areas and to best support the overall site circulation and needs. |
| Residential Density: | N/A |
| Parking and Access: | Individual access to be provided from side streets, when applicable, with on-site parking requirements to align with the proposed use. |
| Other Considerations: | This district promotes the maintenance and enhancement of the existing uses. |

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Residential-Only Districts

The Residential-Only District represents existing residential development within the western portion of the corridor. Today, residential development varies from apartments to single family homes on lots of 1 acre or larger. Future land use is guided for high density residential, low density residential, and one-acre single family. These uses are all grouped into a single Neighborhood Residential District for the purposes of the Wayzata Boulevard Corridor Study.

Neighborhood Residential

The Neighborhood Residential District is intended to support existing residential development at all densities. Residential development has been the primary focus of this area of the corridor and is guided to remain for years to come.

| | |
|------------------------------|---|
| Guided Uses: | Single-family and multi-family residential. No new commercial uses will be permitted. |
| Building Height: | Maximum of three stories |
| Building Orientation: | Development should be guided to align with existing development forms in the neighborhoods. Commercial uses should respect adjacent residential uses and fit within the context. For example, a consistent setback across the entire district provides a similar look and feel for travelers as they move along the corridor. |
| Residential Density: | 1 to 40 units per acre in alignment with the future land use plan guidance. This category is used to represent four land use categories within the 2040 Comprehensive Plan, including One Acre Single Family (0.5 to 1 unit per acre), Low Density Residential (1 to 3 units per acre), Medium Density Residential (6 to 12 units per acre) and High Density Residential (12 to 40 units per acre). Bonus densities may also be available as allowed within the zoning ordinance. |
| Parking and Access: | Individual access to be provided from side streets with parking requirements to align with the proposed use. |
| Other Considerations: | This district promotes the maintenance and enhancement of the existing neighborhoods. |

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SAFETY IMPROVEMENTS



What are Safety Improvements?

Short-term improvements that specifically support the safety of all travelers – walking, biking, rolling, driving, etc.

Common Engagement Themes

Bicycle and pedestrian safety: The ability to move safely throughout the corridor was a primary theme.

Crash rates and driving conditions: Community members recognize common crash areas and situations that can be refined through future improvements.

Safety Improvement Framework Elements

Short-Term Improvements; Spot Improvements

Safety improvements were explored within this study as an opportunity for an initial step of implementation. A desire for increased safety measures was heard loud and clear through engagement and supports local and regional goals. This safety improvement framework explores high priority areas for improvements through technical analysis and engagement. A total of six locations were then explored to identify specific recommendations that can be implemented in the short term and are discussed in greater detail within each Gateway Area. The six locations included the Wayzata Boulevard intersections with Ferndale Road, Barry Avenue, Broadway Avenue, Superior Boulevard, Central Avenue, and Bushaway Road.



Bicyclist on Wayzata Boulevard in the West Gateway

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TRANSPORTATION AND MOBILITY



What is Transportation and Mobility?

Safety improvements include elements that support the movement of people and goods throughout the corridor and to their destination.

Common Engagement Themes

Multimodal infrastructure: The lack of consistent and connected multimodal infrastructure is recognized by the community.

Varied needs and uses: The corridor supports a variety of transportation needs for users, from through movements to short trips to corridor destinations, and each user has their own desire for the future of the corridor.

Safety and driver confusion: The existing design through some segments of the corridor creates confusion for some users, creating safety or access issues. For example, the center left-turn lane along the eastern portion of Wayzata Boulevard is used for a variety of purposes outside of its intended use for left turns and queuing. These other uses include a passing or bypass lane for those traveling along Wayzata Boulevard or an acceleration lane for those turning onto the corridor.

Transportation and Mobility Improvement Framework Elements

Design improvements
Multimodal infrastructure
Spot improvements

While this study explores a nearly two-mile stretch of roadway, the characteristics, design, and context vary greatly across the relatively short distance. The transportation and mobility framework sets out to explore a balance of mobility (access to destinations), ease of travel, and overall safety for all modes. The corridor strategies specifically focus on [Guiding Principles 1, 2, and 8](#) to establish a sense of place and create a safe, convenient, and pleasant experience for all.

The transportation and mobility strategies are supported by the safety improvements identified within this Plan. Generally, the transportation and mobility recommendations focus on broader, long-term improvements to the roadway to improve the overall transportation experience, rather than emphasis on specific safety improvements.

The improvements and solutions outlined within the following pages include two primary components – design modifications (i.e., what elements are included in the roadway) and specific spot improvements that support the redesign. While the specific recommendations are identified in the following pages, there are a few elements that apply to all.

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- **Planning Level Recommendations:** The recommendations and design solutions presented for each segment represent planning level concepts. This means that specific dimensions and design details will be refined as part of a formal design process prior to construction. Dimensions are provided on the proposed cross-sections to represent a planning level design that would be refined through further design of the roadway. For example, a sidewalk can range from 4-feet to 10-feet in width, depending on the context and needs, and details will be refined through the design process.
- **Design Refinements:** The solutions and strategies identified will take time to implement – from final design to the identification of funding through construction. It should be recognized that design refinements will be made to respond to the changing context and needs.
- **Cross Section Context:** The existing and proposed cross-sections presented for each segment are intended to represent a general condition of that segment of Wayzata Boulevard. It should be noted that the cross section will vary in actual implementation as turn lanes and other elements are included to fit the needs of the specific context.

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PUBLIC REALM IMPROVEMENTS



What are Public Realm Improvements?

The public realm is the general space, from building edge to building edge that we occupy as we travel from place to place, regardless of mode. It includes the roadway, boulevards, sidewalks, and front yards.

Common Engagement Themes

Gateway: The corridor is a gateway into Wayzata at three key locations, and the ability to emphasize this characteristic is desired.

Sense of Place: Wayzata Boulevard is a corridor with many characteristics, the development of a unified sense of place is desired to support the charming community.

Public Realm Improvement Framework Elements

Public realm framework; District setbacks; Wayzata gateways; Public art installations

WHAT IS THE PUBLIC REALM?

The public realm is the conduit for connecting the destinations within the community and generally includes the road and all areas from building edge to building edge, including both public right of way and private front yards. Private front yards contribute to the overall viewshed, but the activation of this space is limited to commercial front yards as private homes are not open to all. The overall size and scope of the public realm varies greatly throughout a community, as roadways vary in size and front yard setbacks differ by zone. Regardless of size, the public realm shapes the view and experience of a place.

The public realm is perceived differently by drivers, bicyclists, and walkers, and consideration must be given to the viewshed of each type of user. Drivers experience the public realm through a windshield at speed and are also focused on their safety and mobility. Bicyclists and pedestrians experience the public realm from a broader perspective, but with more detail due to the slower speed of travel.

The public realm can include several elements and spaces, from driving lanes to boulevards to gateway spaces. Each element builds from each other to shape the overall experience – *How much of the public realm is dedicated to transportation; How is greenspace prioritized; How tall are buildings and is there a consistent design style.*

Influence of the Public Realm

The public realm is a major element of what makes a place great. It provides opportunities for social connection, links destinations, establishes the image or identity, and hosts activities and uses. From that perspective, the public realm is the foundation for how residents and visitors experience a place or space.

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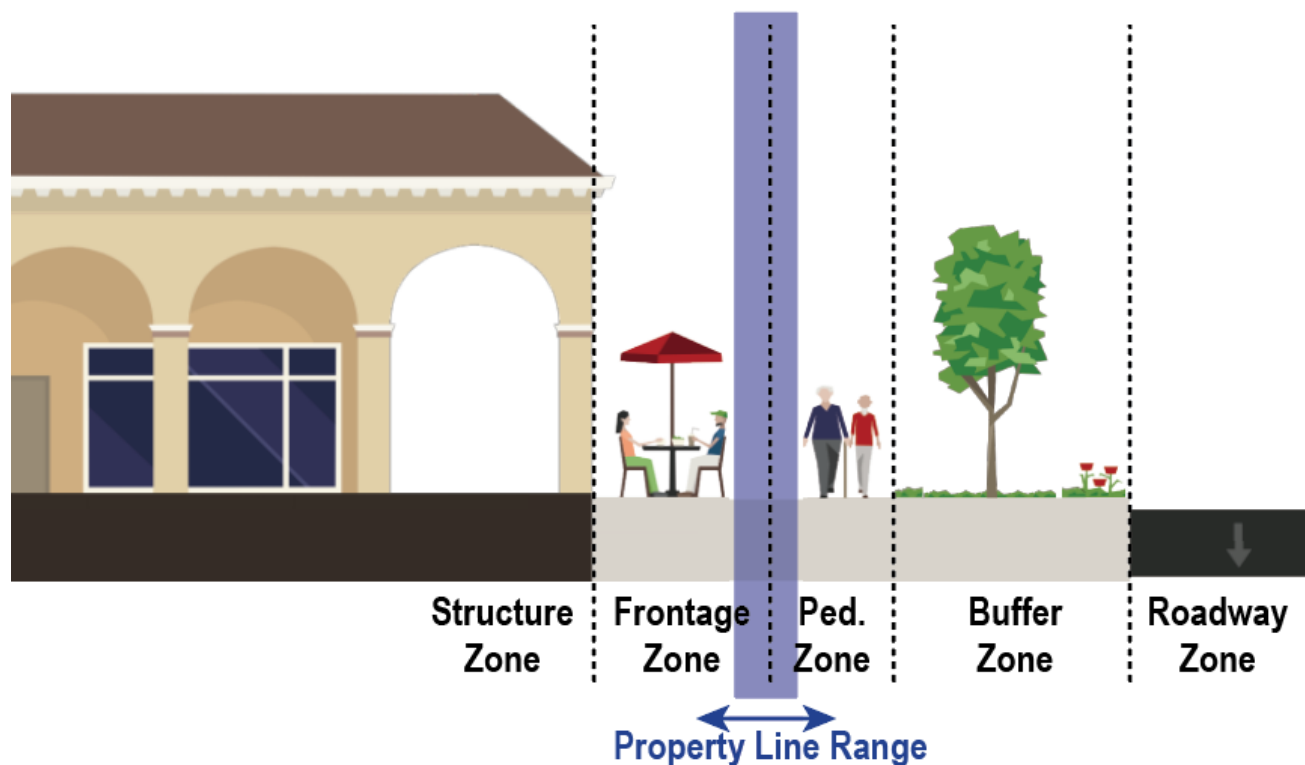
Public Realm influences the response to the following questions: What do you see? How do you interact? Where can you gather and socialize? How can you access destinations? How do you feel – welcome, safe, vulnerable, etc.? What is the identity of the area?

These questions and perspectives can be used to assess and analyze future improvements and guidelines for the public realm.

The public realm is influenced by many factors – roadway changes, setbacks, and design standards – that shape the overall streetscape. To capitalize on the opportunities identified for the Wayzata Boulevard public realm, policy recommendations were explored to establish a framework that can be individualized by land use or geographical sub area.

This framework begins with recognizing the elements of the public realm and how they correlate to one another, ensuring that the public realm serves a primary function of mobility and connection while also supporting socialization, identity, and activities or uses. For the purposes of this framework, the public realm is split into two halves along the centerline or middle of the roadway.

This public realm space is categorized into five zones. These zones provide specific functions within the space, creating unique opportunities and considerations for each:



Property lines can fall between fall between the Frontage and Pedestrian Zones

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| Zone | Descriptions | Opportunities and Elements |
|------------------------|--|---|
| Structure Zone | The Structure Zone recognizes the limits of the public realm – the structures. While the public realm does not extend into the interior, the exterior building form and materials influence the public realm. The location of existing buildings is fixed, however the vision for the Structure Zone anticipates a difference balance of the Structure and Frontage Zones if properties redevelop. | <ul style="list-style-type: none"> • Wayfinding • Gathering spaces |
| Frontage Zone | The Frontage Zone includes elements of the public right of way and private property. This zone can include a number of elements from public furnishings to private uses, including parking, cafes/patios, and landscaping. This can be a key zone for activation of the public realm. | <ul style="list-style-type: none"> • Accessible connections • Corridor lighting • Public art • Wayfinding • Gateway signage • Refuge and rest areas • Gathering spaces • Greenspaces and vegetation |
| Pedestrian Zone | Pedestrian Zone is dedicated to the movement of people by non-vehicular modes. Sidewalks and trails are the primary infrastructure for moving those walking and rolling and also expand to bicyclists when wide enough. Similar to the Roadway Zone, this zone promotes movement and connection within the public realm. | <ul style="list-style-type: none"> • Accessible connections • Public art • Wayfinding • Transit shelters and waiting areas |
| Buffer Zone | The Buffer Zone provides a buffer to separate pedestrian and vehicular movement, addition of green space, and elements that can slow traffic. | <ul style="list-style-type: none"> • Accessible connections • Corridor lighting • Wayfinding • Gateway signage • Greenspaces and vegetation • Buffers and separation |
| Roadway Zone | The Roadway Zone is dedicated to the infrastructure that moves vehicles and bicyclists. This zone includes the roadway starting at the curb. | <ul style="list-style-type: none"> • Accessible connections • Wayfinding • Transit |

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There are specific improvements and elements that can be considered within each zone to build the overall experience. These elements include:

- **Accessible connections** to move people through the public realm and connect them to destinations.
- **Corridor lighting** at the pedestrian scale that increase safety and visibility.
- **Public art** that enhances the identity of the community and corridor. Public art can include permanent installations, temporary fixtures, and ephemeral art.
- **Wayfinding** signage and tools that support connection and guide travelers to destinations within the community.
- **Gateway signage** that helps to solidify the entrance to the City of Wayzata as people enter the community. This signage is a key element of establishing identity.
- **Refuge and rest areas** provide spaces for people to rest and relax in the public realm between destinations and experiences. This can include benches for rest or refuge spaces that allow travelers to pause and take in the space.
- **Gathering spaces** that provide a space for social connection within the corridor.
- **Greenspaces and vegetation** provide multiple functions, including beautification, traffic calming, and separation.
- **Buffers and separation** support a feeling of safety between competing uses and needs.
- **Transit** offerings and infrastructure support another mode of movement throughout the corridor.

District Setbacks

The front yard setback of the properties along a corridor influences the public realm experience as it establishes the available space within the Frontage Zone. The existing setbacks within the study area were explored, examining both the qualitative experience as a pedestrian or driver and the overall space needs to accommodate various uses. As a result, setback recommendations were established for the study area using the Wayzata Boulevard and Bluff Districts created within the *Design Standards*.

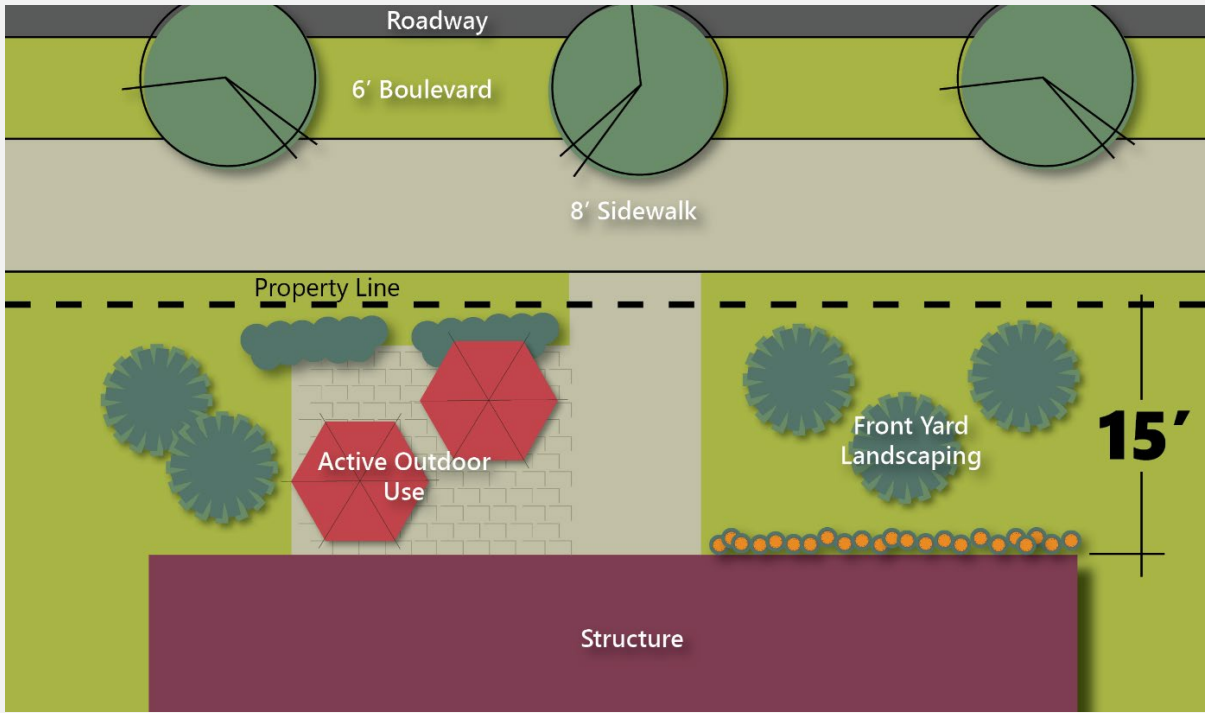
Wayzata Boulevard District Setbacks

Existing setbacks within the Boulevard District range from a minimum of 10' to greater than 50' throughout the study area. This range of setbacks creates various experiences with some properties providing active seating areas within the front yard to large parking lots situated between the building and the front lot line. Setback recommendations for properties with frontage along Wayzata Boulevard within the Boulevard District were developed in a tiered system – creating flexibility for the property owner to provide varied uses within the front yard. The shorter setback guidance aligns with the design standards, supporting activation of the frontage zone. However, this narrower area does not allow for front yard parking and limits available area for other active uses. A larger setback is also proposed that could accommodate a

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single row of parking in the front yard. These setback recommendation vary from the currently adopted design standards and amendment would be needed to allow the system identified below.

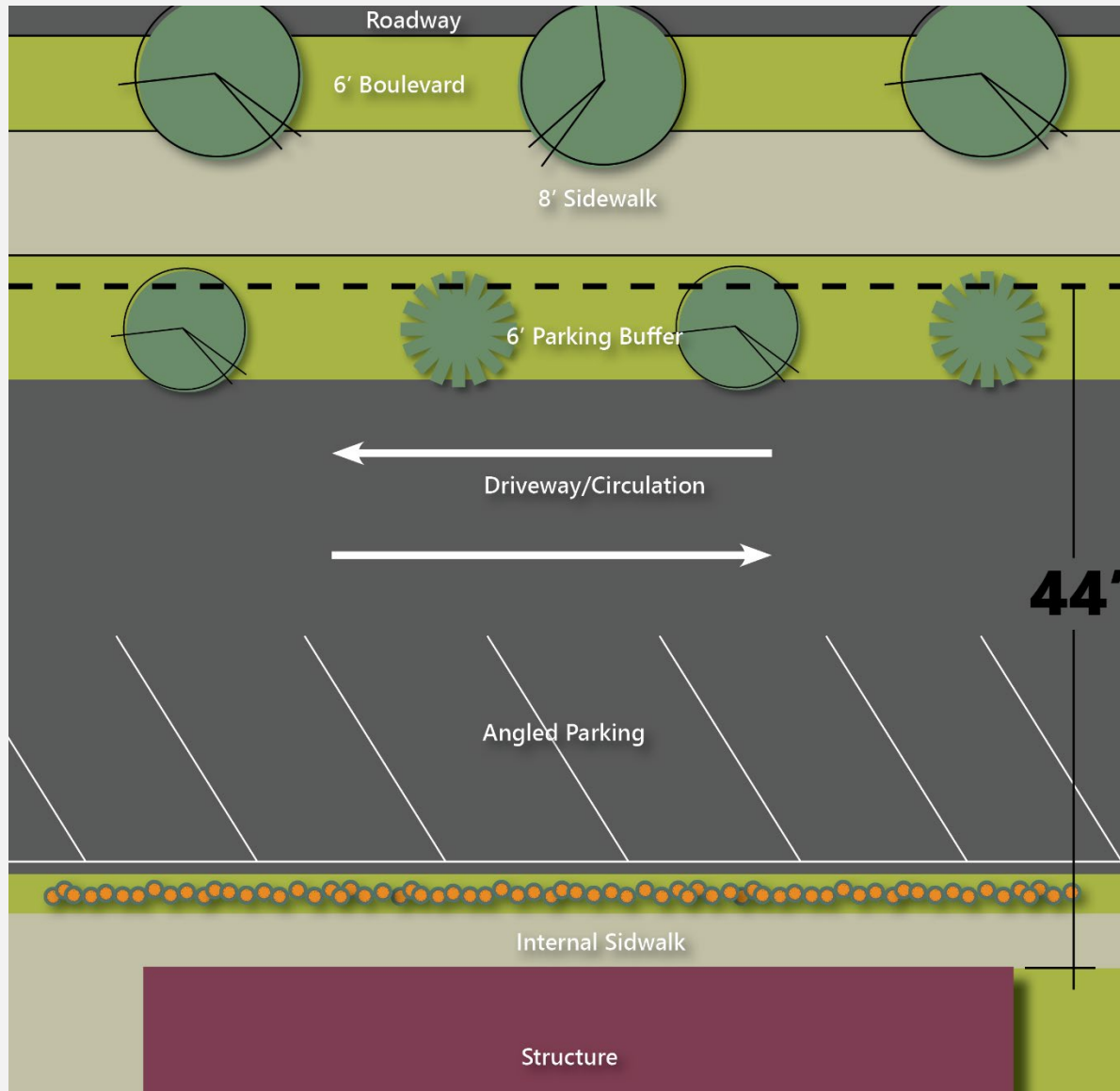
Tier 1: Narrow Setback



The 15-foot setback provides the minimum space needed to support active uses with easy views of the uses within the building. Parking would not be allowed within this setback tier, and uses could be limited to outdoor seating, public uses, and general landscaping.

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Tier 2: Wide Setback



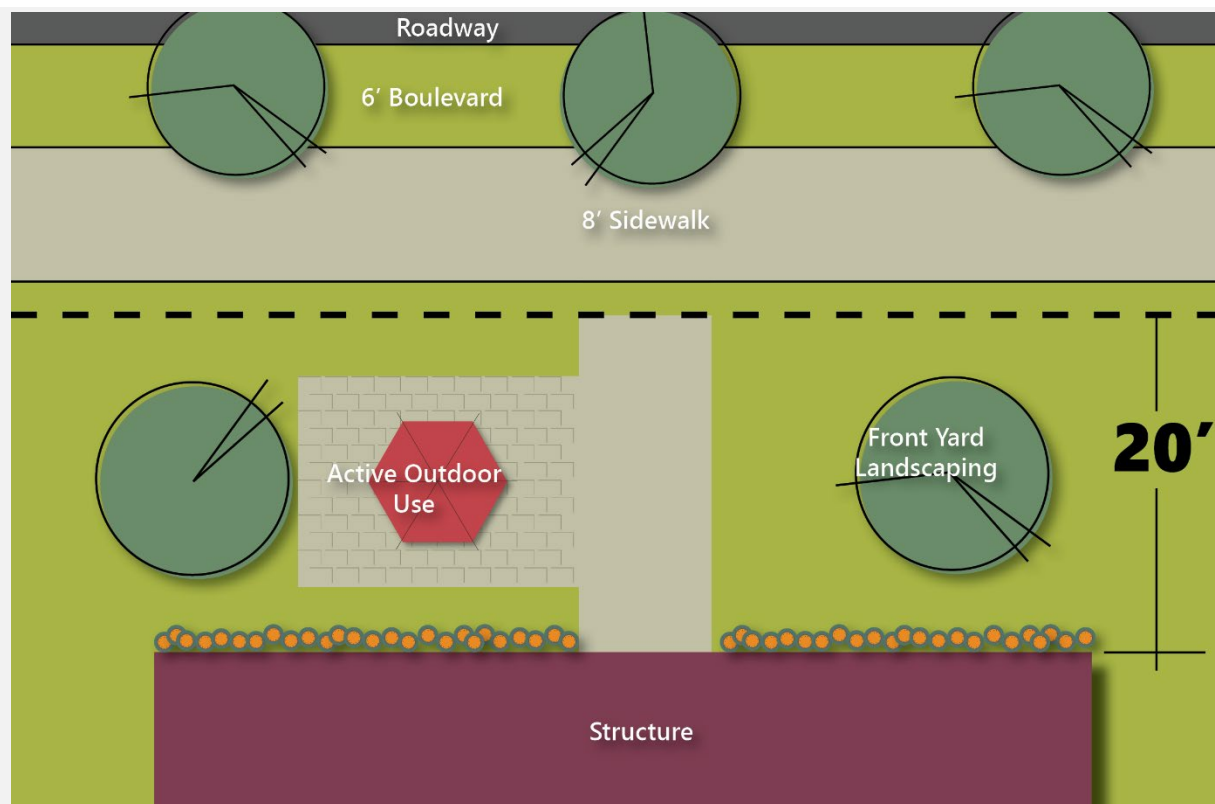
The 44-foot setback provides the additional space to expand the active uses or accommodate parking within the front yard. Additionally, this distance keeps the structure within an adequate distance for view by travelers along the corridor. A single row of parking could be included within this tier, with additional parking located in the side or rear yard as needed. Parking and circulation within the front yard would require the planting of a 6' parking buffer between the front lot line and parking areas. The circulation/driveway width, and overall setback, could be reduced if one-ways across multiple parcels are feasible and pursued.

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Bluff District Setbacks

Existing setbacks within the Bluff District range are generally 20' for existing residential areas. Larger setbacks are used for public uses within the district as well. The general 20' setback maintains a consistent experience as you travel along the corridor, and this experience should be maintained as new development is pursued. A single setback of 20' is recommended within the Bluff District to maintain the current look and feel. This space allows for a combination of active outdoor use and landscaping. For single-family homes with driveway access to Wayzata Boulevard, front yard parking would be allowed. However, no front yard parking should be allowed for multi-family or commercial uses.

Bluff District Setback



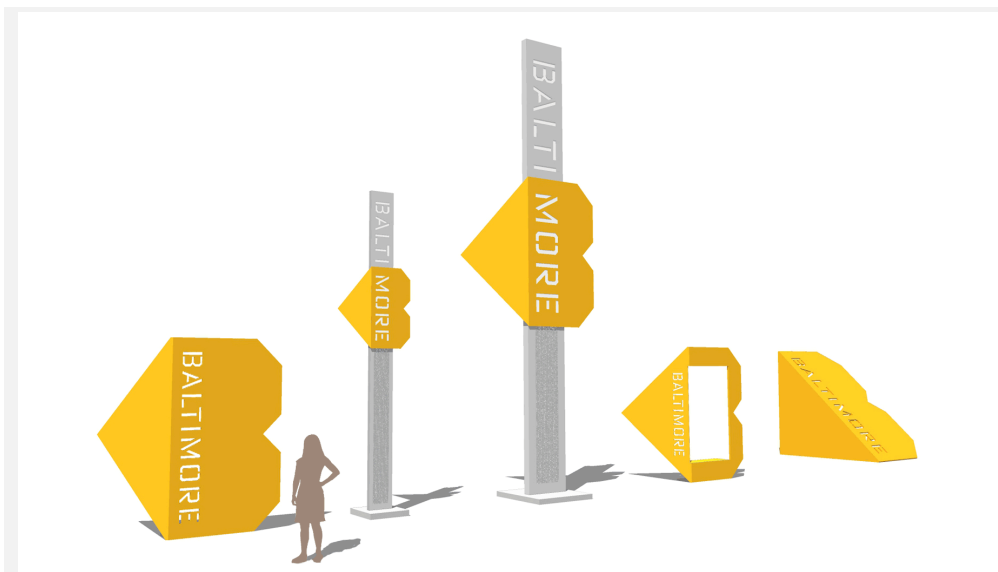
The 20-foot setback provides the minimum space needed to support active uses with easy views of the uses within the building. Parking would only be allowed for single family homes with driveways that access Wayzata Boulevard, and uses could be limited to outdoor seating, public uses, and general landscaping.

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Gateway Signage and Public Art

The roadway design and adjacent development are fixtures of the look and feel of the public realm. The addition of signage, wayfinding and public art can further help people identify and navigate where they are and create a sense of place. Signage and public realm fixtures should be designed to support the desired identity of the area and support the overall image of the community. Additionally, these elements should inform travelers of destinations and support directional information. Wayzata will need to identify the overall look and feel for gateway signage along Wayzata Boulevard that is representative of the city's charm and sense of place. The following examples represent varied strategies deployed in other cities that could be explored as the city works toward implementation.

Baltimore, Maryland



The City of Baltimore explored gateway and wayfinding signage opportunities for use throughout the City. The resulting design included public art components to establish a unified sense of place.

Source: Public Mechanics

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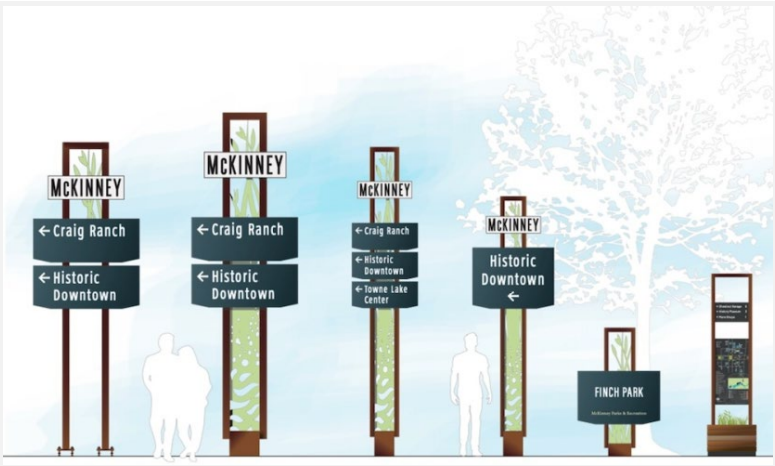
Gainesville, Georgia



The City of Gainesville installed community gateway signage at key locations in the community. The design of the signage reflected an overall design and aesthetic that reflects the overall city's design choices, incorporating elements that are reflective of the community. Additionally, the sleek profile provided a varied silhouette from the traditional monument sign.

Source: Gainesville Times

McKinney, Texas



The City of McKinney created a gateway and signage plan that included a system of signs to be placed through the community. Signs were created for specific destinations along with additional wayfinding signage to direct people throughout the community.

Source: Community Impact

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WAYZATA EAST GATEWAY PLAN

The East Gateway includes Wayzata Boulevard from Superior Boulevard to Bushaway Road. This segment of Wayzata Boulevard is under the jurisdiction of Hennepin County.

LAND USE AND DEVELOPMENT FRAMEWORK

There are several established uses within the East Gateway area. Development opportunities are specifically focused where the conditions and context would like support public or private investments.



Land Use Districts

A number of land use districts were identified within the East Gateway area. In addition to the corridor-wide recommendations identified, the following characteristics should be considered as development is pursued.

| | |
|---------------------------------------|---|
| Office, Service, and Mixed-Use | Office, Service, and Mixed-Use creates opportunities to support the existing land uses along the southern edge of Wayzata Boulevard with the opportunity to introduce residential uses through redevelopment. |
| High Intensity Mixed-Use | The High Intensity Mixed-Use district provides a key opportunity for larger scale redevelopment of the study area. Larger redevelopment opportunities could support the creation of housing at a variety of scales in coordination with commercial redevelopment. |
| Auto Oriented Commercial | Auto Oriented Commercial uses support the existing service uses along the corridor. This district was specifically created to support the maintenance of these core services for the community, with opportunities for redevelopment as needed. |
| Public Spaces | The Preserve is the only semi-public use within the East Gateway. No major redevelopment is anticipated. |
| Parks and Open Spaces | There are a number of parks and greenspaces on or near the East Gateway. Efforts to maintain these resources and amenities should be emphasized along with the opportunity to provide physical connections to these spaces for the community. |

Land Use Opportunities

A single site has been identified as a land use opportunity for future public and/or private investment.

| Site | Address | Size | Description |
|-----------------|--|------------|---|
| Colonial Square | 1125 Wayzata Boulevard 1101 Wayzata Boulevard | 7.92 acres | The overall size and amount of frontage of the site make it a prime opportunity for redevelopment within the High Intensity Mixed-Use Category. This plan would support the development of vertical mixed-use development of up to four stories on this site in an effort to support continued commercial service offerings and the introduction of residential uses. |

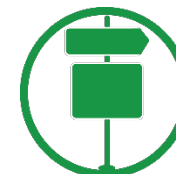
PART C: FRAMEWORK & CORRIDOR STRATEGIES

Connections

Connections to the Big Woods Preserve, North Woods Nature Center, and Heritage Park within this segment create opportunities to connect the community and visitors with quality natural resources within the community. Heritage Park is located directly on the corridor with existing access established. The opportunity to establish formal connections to the Big Woods Preserve and North Woods Nature Center could be pursued through future redevelopment projects or public investment.

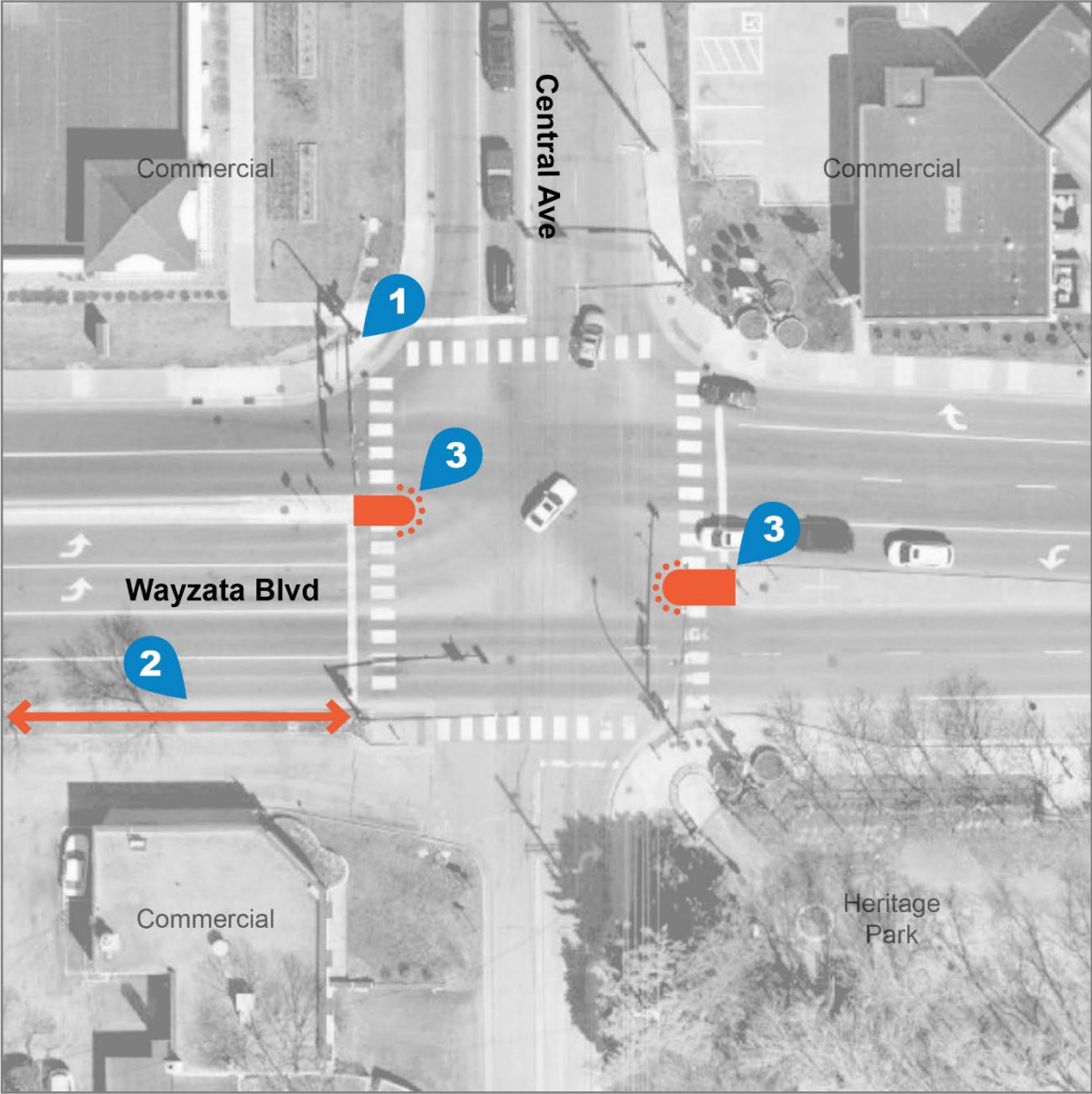
SAFETY IMPROVEMENTS FRAMEWORK

Three intersections with Wayzata Boulevard (Central Avenue, Birch Bend Lane, and Busahway Road) were identified for short term improvements based on past summaries crash history and the existing intersection design. These improvements are relatively low impact and could be implemented in the near term. Longer-term safety improvements are identified with the Transportation and Mobility strategies.



PART C: FRAMEWORK & CORRIDOR STRATEGIES

Central Avenue and Wayzata Boulevard

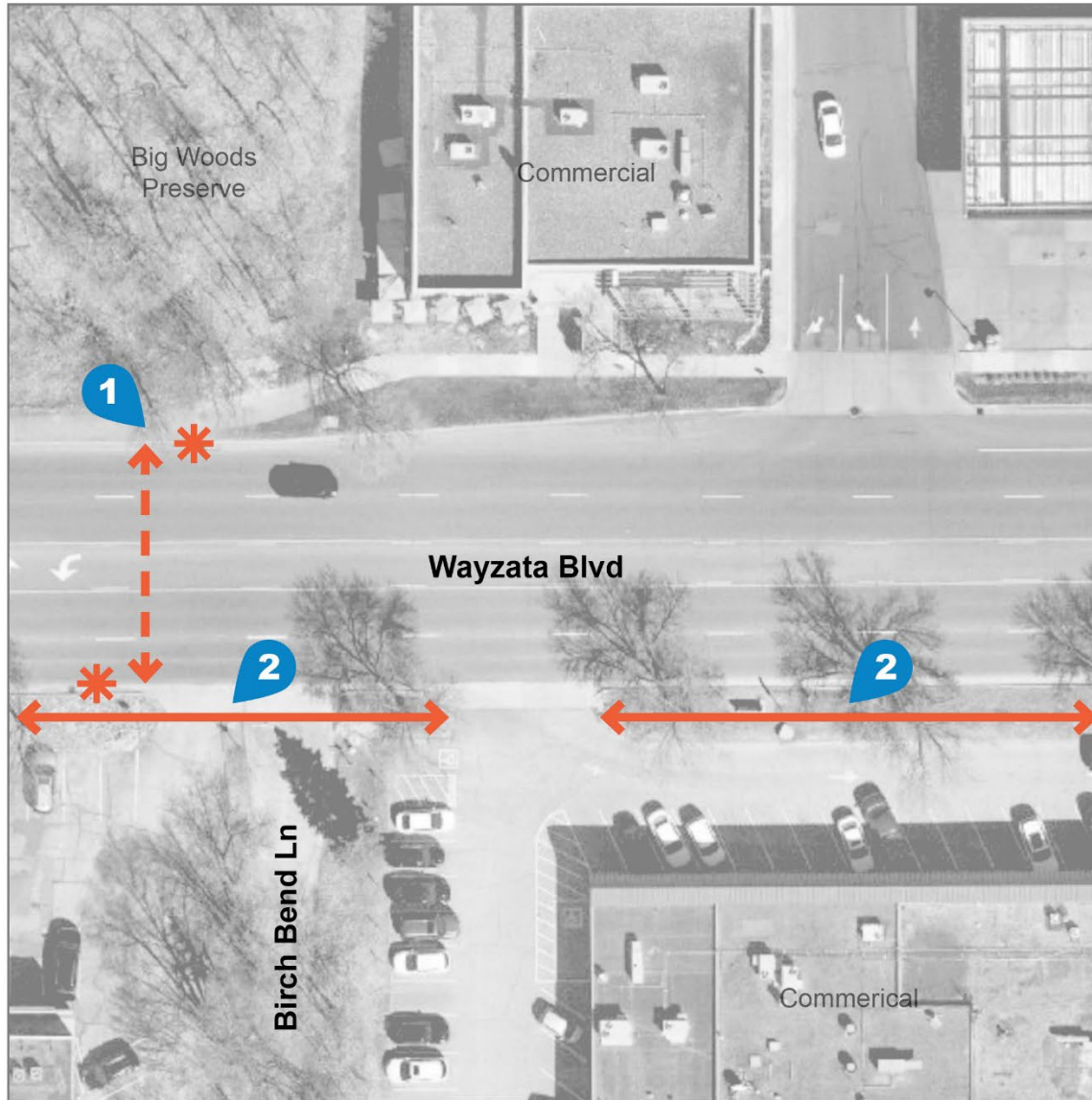


- 1 Signal Timing Upgrade**
Incorporate leading pedestrian intervals and increased crossing times into signals to increase pedestrian visibility and to allow more time for crossing.
- 2 Close Sidewalk Gaps**
Close sidewalk gaps on south side of Wayzata Blvd.
- 3 Add Curb Extensions**
Add curb extensions to pedestrian refuge islands to improve visibility of pedestrians and to provide a buffer between pedestrians and turning traffic.



PART C: FRAMEWORK & CORRIDOR STRATEGIES

Birch Bend Lane and Wayzata Boulevard



1 Add RRFB (Rectangular Rapid Flashing Beacon) Crossing

Add a mid-block crossing with a rectangular rapid flash beacon to improve pedestrian connectivity and visibility and to lower vehicular speeds between Central Ave and Bushaway Rd.

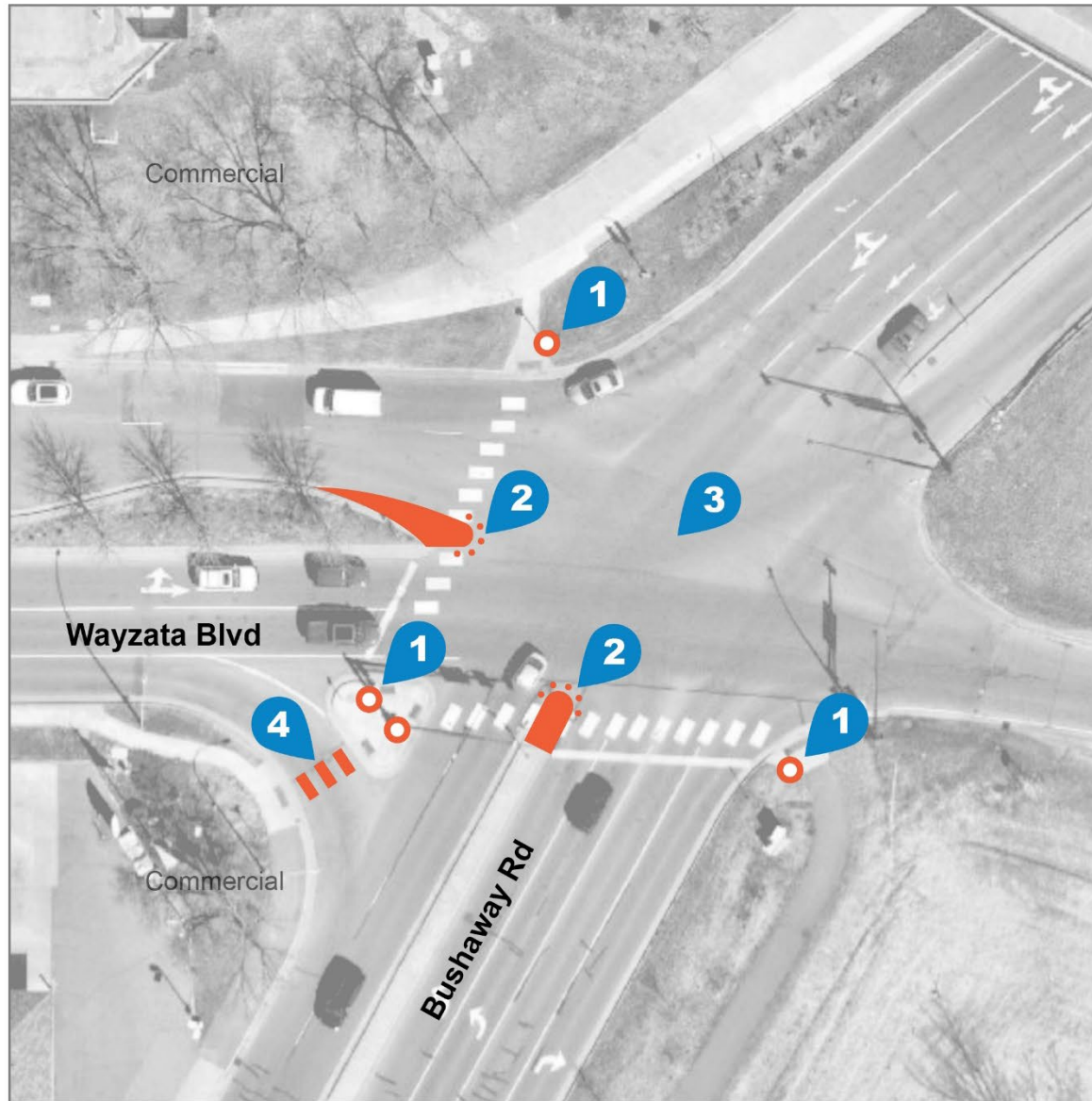
2 Close Sidewalk Gaps

Close sidewalk gaps on south side of Wayzata Blvd.



PART C: FRAMEWORK & CORRIDOR STRATEGIES

Bushaway Road and Wayzata Boulevard



- 1 Move Crosswalk Buttons and Update Quadrants**
Separate crosswalk buttons and update push button landings and ramps to meet ADA compliance.
- 2 Add Curb Extensions**
Add curb extensions to pedestrian refuge islands to improve visibility of pedestrians and to provide a buffer between pedestrians and turning traffic.
- 3 Signal Timing Upgrade**
Incorporate leading pedestrian intervals and increased crossing times into signals to increase pedestrian visibility and to allow more time for crossing.
- 4 Stripe Crosswalk**
Stripe existing crosswalk to increase pedestrian visibility.



PART C: FRAMEWORK & CORRIDOR STRATEGIES

TRANSPORTATION AND MOBILITY FRAMEWORK



The East Gateway has had a long history of supporting between 15,000 to 20,000 vehicles per day. The existing design includes two travel lanes in each direction with a center left turn lane or raised median diving travel lanes. Generally, these wide travel lanes support higher speeds as drivers move through the area. There are sidewalks on both the north and south side of the roadway with small gap areas that limit connectivity. While sidewalks are present, the number of individual access points and volumes of these access points creates safety concerns for pedestrians with turning vehicles. Dedicated pedestrian crossing infrastructure is limited to the intersections with Central Avenue and Bushaway Road. The following table details the existing design elements within the East Gateway.

Future traffic volumes were explored, accounting for both identified redevelopment opportunities and assumed traffic growth. 2040 traffic volumes are forecasted to grow to 25,000 vehicles per day, an increase of 29 percent over 19 years. Both the existing volumes and forecasted volumes were used to analyze recommendations for this segment.

| Roadway | # of Lanes | Speed Limit | Center Turn Lane/Median | On-street parking | Multi-modal Infrastructure | Sidewalks or trail | Traffic volume (Year) |
|-----------------------|------------|-------------|--|-------------------|----------------------------|------------------------------------|-----------------------|
| Existing Wayzata Blvd | 4/5 | 35 mph | Center Left Turn Lane or Raised Median | None | Bus stops with Benches | Sidewalks with gaps on either side | 19,339 (2021) |

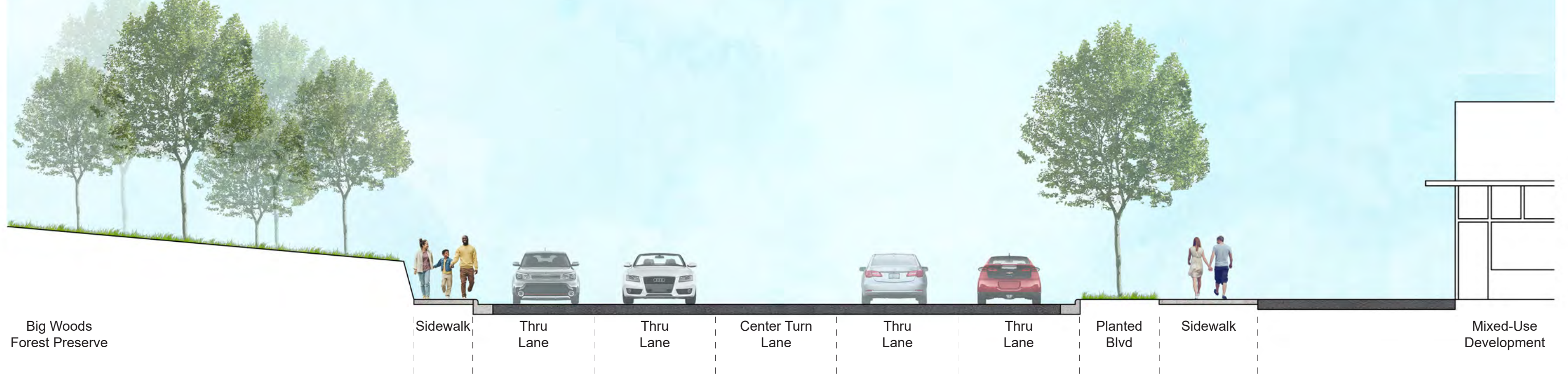
Design Recommendations

Opportunities for the East Gateway focused on balancing mobility with safety and access improvements, creating opportunities to establish a sense of place, and enhanced multimodal infrastructure. The design modifications maintain the four travel lanes but include the introduction of a center median. This center median serves multiple functions – aesthetics, traffic calming, and access limitations. Recommendations are also included to support bicycle and pedestrian connections. There are several sidewalk gaps that can be closed.

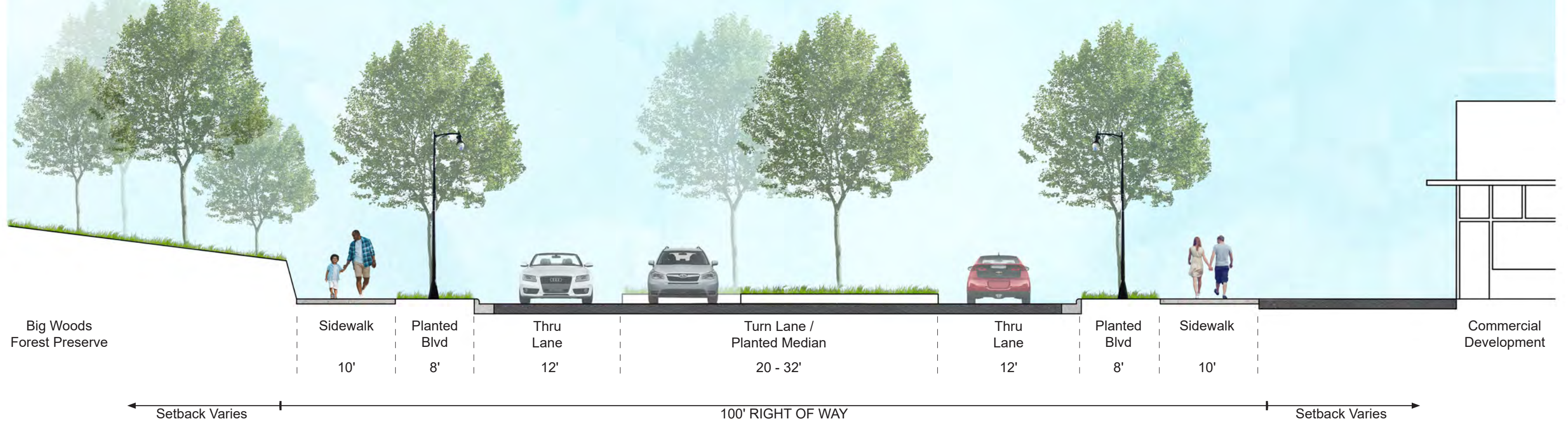
| Roadway | # of Lanes | Speed Limit | Center Turn Lane/Median | On-street parking | Multi-modal Infrastructure | Sidewalks or trail |
|-----------------------|------------|-------------|--|-------------------|----------------------------|---------------------|
| Proposed Wayzata Blvd | 4 | 30 mph | Planted Median with dispersed turn lanes | None | Bus stops with Benches | Continuous Sidewalk |

Wayzata East Gateway

EXISTING SECTION C

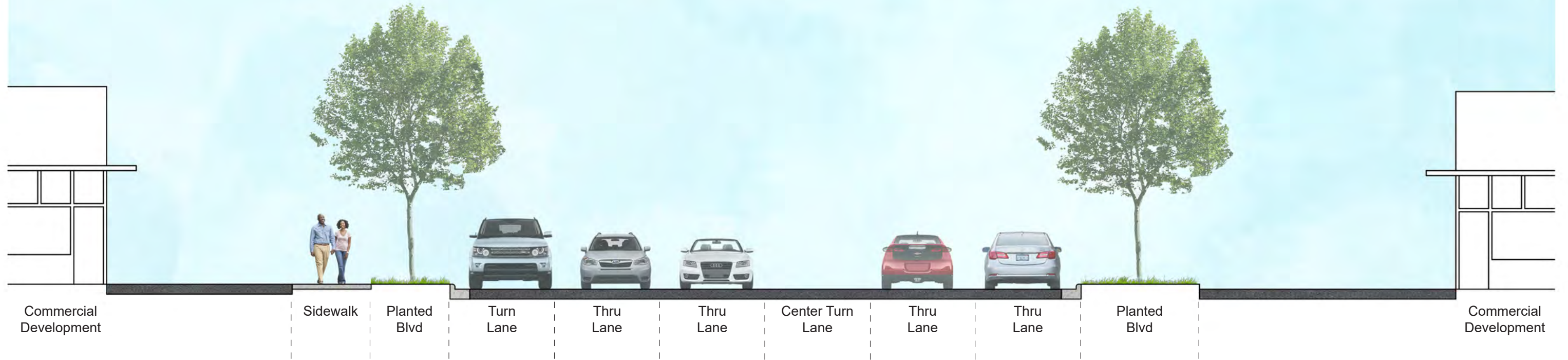


PROPOSED SECTION C

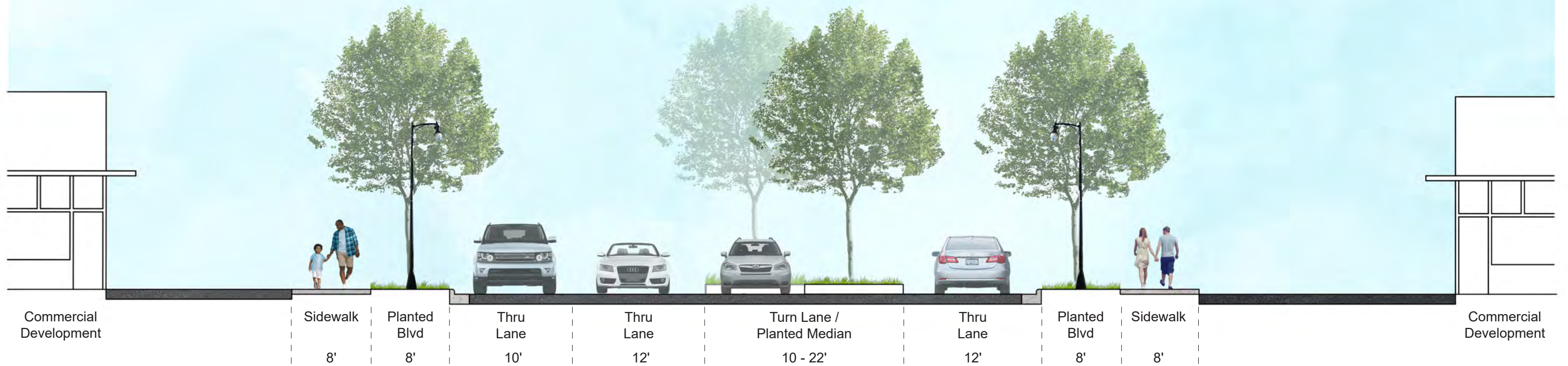


Wayzata East Gateway

EXISTING SECTION D



PROPOSED SECTION D



← Setback Varies | 100' RIGHT OF WAY | Setback Varies →

PART C: FRAMEWORK & CORRIDOR STRATEGIES

Spot Improvements

In addition to the overall design updates, location specific strategies were also identified to support the corridor vision, mobility, and sense of place. The specific identified improvements include:

| | |
|--|--|
| Central Avenue and Wayzata Boulevard | <p>Add Curb Extensions: Add curb extensions to pedestrian refuge islands to improve visibility of pedestrians and to provide a buffer between pedestrians and turning traffic.</p> <p>Signal Timing Upgrade: Incorporate leading pedestrian intervals and increased crossing times into signals to increase pedestrian visibility and to allow more time for crossing.</p> |
| Midway Crossings near Preserve Entrance | <p>Midway Crossing: Incorporate midway crossing via roundabout or J-turn at entrance to Big Woods Preserve.</p> |
| Midway Crossing near Birch Bend Lane | <p>Midway Crossing: Incorporate midway crossing via roundabout or J-turn at Birch Bend Intersection.</p> |

Bicycle and Pedestrian Infrastructure

The closure of sidewalk gaps throughout the East Gateway area is a key implementation element of the transportation and mobility recommendations. The overall width of the sidewalk within this area should be refined in conversations with Hennepin County and City of Wayzata through the design process. For example, the inclusion of 10-foot sidewalks would create the opportunity for the sidewalks to serve as shared use path that accommodate pedestrians and bicyclists.

PUBLIC REALM IMPROVEMENTS FRAMEWORK

The public realm is relatively consistent through the East Gateway, including primarily commercial uses. The average setback of these commercial uses is similar through the auto oriented uses and increase to the west in the office and service area. Overall, these setbacks and commercial uses create opportunities to activate the frontage zone of the public realm with public and private investments. The public realm considerations for each of the public realm zones are identified within the table below.

| Zone | East Gateway Considerations | East Gateway Recommendations |
|-----------------------|--|--|
| Structure Zone | <p>The Structure Zone includes primarily commercial structures that must align with the community design standards which creates a unified sense of place through redevelopment. The setback guidance for this gateway would prioritize a 15' setback with public uses and landscaping, while also allowing a larger 40' setback to accommodate one row of parking with landscaped buffer.</p> | <ul style="list-style-type: none"> • Consistent material and standards • Consistent setbacks |

PART C: FRAMEWORK & CORRIDOR STRATEGIES

| Zone | East Gateway Considerations | East Gateway Recommendations |
|------------------------|---|--|
| Frontage Zone | The Frontage Zone is an opportunity for a combination of public and private investments to solidify the sense of place, providing gathering spaces, and provide places of refuge and rest. For example, the existing patio space for the Punch Pizza/Chipotle development creates a welcoming and inviting connection to the Frontage and Pedestrian Zones. | <ul style="list-style-type: none"> • Accessible connections • Corridor lighting • Refuge and rest areas • Gathering spaces • Greenspaces and vegetation |
| Pedestrian Zone | The Pedestrian Zone will be improved through this segment will the infill of sidewalk gaps to complete the overall network. These improvements also create the opportunity to support aesthetic improvements and are they key location for wayfinding signage. | <ul style="list-style-type: none"> • Public art • Wayfinding • Transit shelters and refuge |
| Buffer Zone | The Buffer Zone should be maintained throughout this segment to provide a greenspace buffer between the trail/sidewalk and the roadway edge. This zone may vary in width throughout the section. | <ul style="list-style-type: none"> • Corridor lighting • Wayfinding • Greenspaces and vegetation • Crossing Signage |
| Roadway Zone | The Roadway Zone is dedicated to the infrastructure that moves vehicles and bicyclists. This zone includes the roadway starting at the curb with the improvements described in the Transportation and Mobility Framework. | <ul style="list-style-type: none"> • Planted Median • Crosswalk improvements |

Gateway Areas

The East Gateway is the first experience travelers from the east have into the community. This creates opportunities for gateway signage to emphasize the arrival into Wayzata and establish the overall sense of place. There are multiple opportunities to incorporate gateway signage and infrastructure, including larger signage at the Bushaway Road intersection and banners or other elements in new median areas. Additionally, gateway signage could be introduced throughout the corridor to solidify the arrival in Wayzata.

Public Art and Sense of Place

In addition to specific gateway improvements, other opportunities to introduce public art could be explored through the Pedestrian and Frontage Zones. At a minimum, the addition of planted medians introduces additional vegetation areas with opportunities for art installations. Adjacent public uses also create an opportunity to introduce public art installations or other investments.

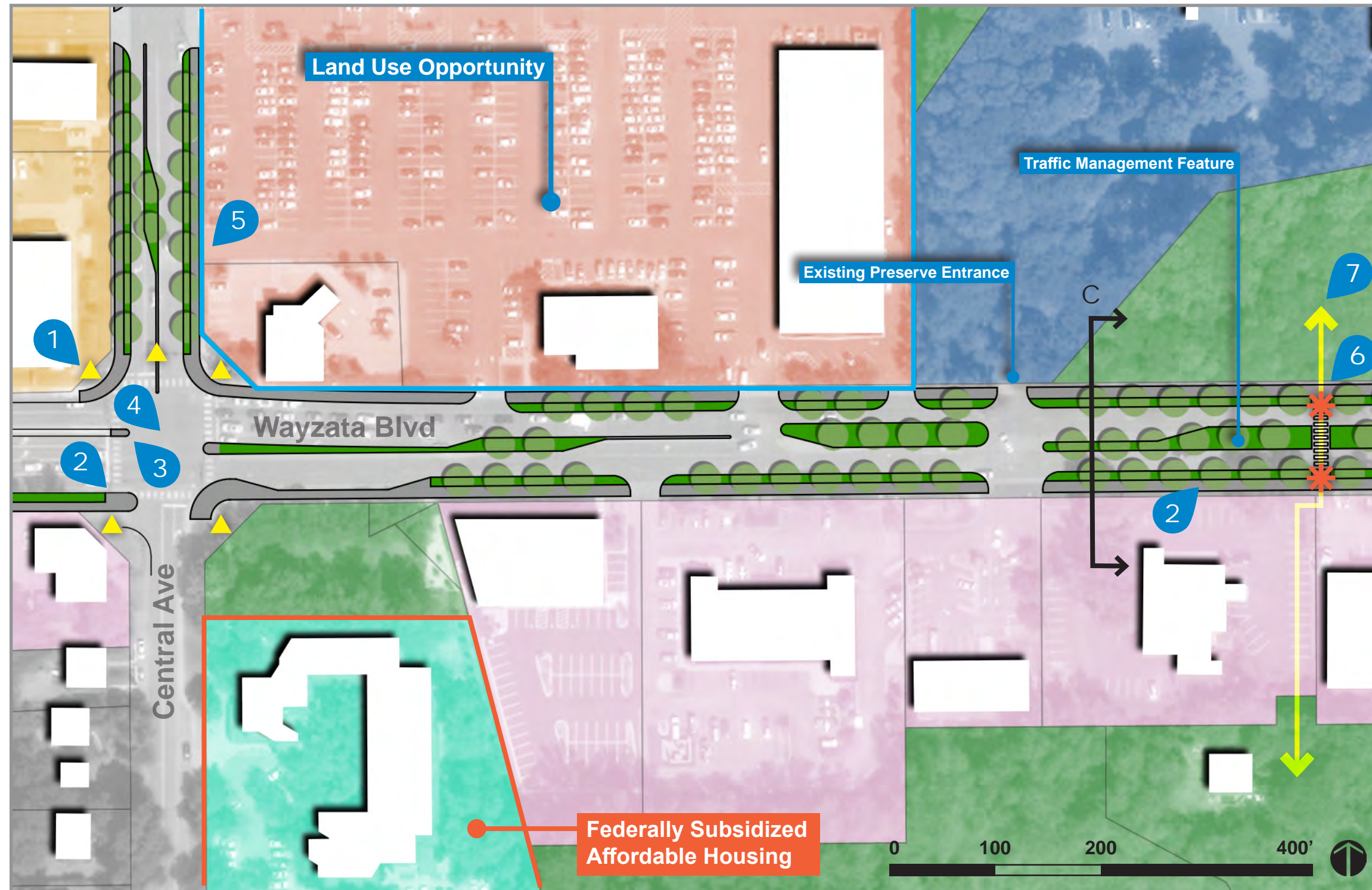
ILLUSTRATIVE PLAN

PART C: FRAMEWORK & CORRIDOR STRATEGIES











The following pages and rendering summarize the framework and strategies identified for the East Gateway. The rendering below represents a graphical illustration of the planning level design identified through this study. Specific design features will be refined through the design process, and modifications from the graphic below are anticipated. This image below is located near the center of the gateway, within the area identified for a traffic management solution (i.e., the oval roundabout), and illustrates a single travel lane in each direction. Four travel lanes are proposed for a majority of this segment outside of the traffic management feature.

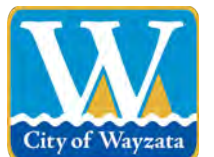


Wayzata East Gateway



- 1 **Gateway Opportunity**
Identify opportunities for gateway signage and banners at Wayzata Blvd and Central Ave.
- 2 **Close Sidewalk Gaps**
Close sidewalk gaps at southwest quadrant of Wayzata Blvd and Central Ave.
- 3 **Add Curb Extensions**
Add curb extensions to pedestrian refuge islands to improve visibility of pedestrians and to provide a buffer between pedestrians and turning traffic.
- 4 **Signal Timing Upgrade**
Incorporate leading pedestrian intervals and increased crossing times into signals to increase pedestrian visibility and to allow more time for crossing.
- 5 **Omit Entrance / Exit**
Omit the entrance and exit to Colonial Square near the intersection of Wayzata Blvd and Central Ave to mitigate circulation conflicts with northbound traffic.
- 6 **Midway Crossing**
Incorporate midway pedestrian crossing via roundabout or J-turn near Big Woods Preserve.
- 7 **Green Space Connection**
Identify future connections between green spaces.

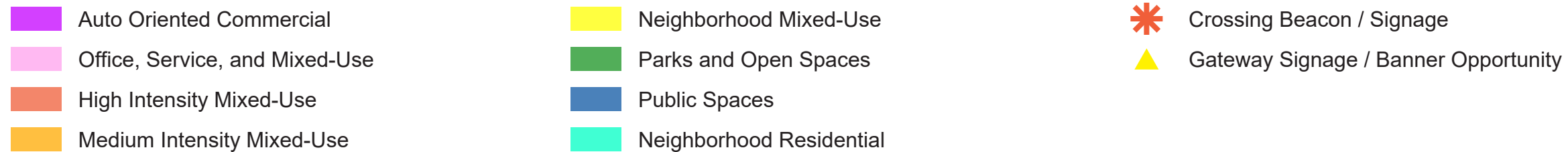
| | | |
|---|--|--|
|  Auto Oriented Commercial |  Neighborhood Mixed-Use |  Crossing Beacon / Signage |
|  Office, Service, and Mixed-Use |  Parks and Open Spaces |  Gateway Signage / Banner Opportunity |
|  High Intensity Mixed-Use |  Public Spaces | |
|  Medium Intensity Mixed-Use |  Neighborhood Residential | |



Wayzata East Gateway



- 1 **Close Sidewalk Gaps**
Close sidewalk gaps on south side of Wayzata Blvd.
- 2 **Future Access Planning**
Identify opportunities for minimizing entrances and exits alongside future developments in the East Gateway to improve traffic flow and safety.
- 3 **Gateway Opportunity**
Identify opportunities for gateway signage and banners at the East Gateway.



PART C: FRAMEWORK & CORRIDOR STRATEGIES

WAYZATA NORTH GATEWAY PLAN

The North Gateway includes all of Central Avenue within the study area. This segment includes the Hennepin County owned roadway.

LAND USE AND DEVELOPMENT FRAMEWORK

There are several established uses within the North Gateway area. Development opportunities are specifically focused where the conditions and context would support public or private investments.



Land Use Districts

Three land use districts were identified within the West Gateway area. In addition to the corridor-wide recommendations identified, the following characteristics should be considered as development is pursued.

| | |
|-----------------------------------|---|
| Medium Intensity Mixed-Use | The Medium Intensity Mixed-Use district identifies opportunities for larger scale redevelopment along the corridor, at a slightly smaller scale than the High Intensity areas. This district was specifically identified for the northwest quadrant of Wayzata Boulevard and Central to identify opportunities for redevelopment that introduces commercial and residential uses. |
| High Intensity Mixed-Use | The High Intensity Mixed-Use district provides a key opportunity for larger scale redevelopment within the study area. Larger redevelopment opportunities could support the creation of housing at a variety of scales in coordination with commercial redevelopment. |
| Neighborhood Mixed-Use | The Neighborhood Mixed-Use district supports the maintenance of existing residential uses along the North Gateway, with the opportunity to introduce neighborhood commercial uses as desired. It is anticipated that the North Gateway will maintain the existing residential uses on the western edge. |

Land Use Opportunities

Two sites have been identified for potential land use opportunities. These are key areas for public and/or private exploration.

| Site | Address | Size | Description |
|-----------------|--|------------|--|
| Colonial Square | 1125 Wayzata Boulevard 1101 Wayzata Boulevard | 7.92 acres | The overall size and amount of frontage of the site make it a prime opportunity for redevelopment within the High Intensity Mixed-Use Category. This plan would support the development of vertical mixed-use development of up to four stories on this site in an effort to support continued commercial service offerings and the introduction of residential uses. The site currently holds a number of commercial uses that provide personal services and retail shopping. Banks, grocery stores, restaurants, and retail uses have been located on these two lots for many years. |

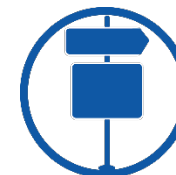
PART C: FRAMEWORK & CORRIDOR STRATEGIES

Connections

Connections to the Big Woods Preserve within this segment create opportunities to connect the community and visitors with quality natural resources within the community. While Big Woods does not have direct frontage onto Central Avenue, the opportunity to establish formal connections to the Big Woods Preserve could be pursued through future redevelopment projects or public investment.

SAFETY IMPROVEMENTS FRAMEWORK

The Central Avenue and Wayzata Boulevard intersection was identified for short term improvements based on crash history and the existing intersection design. The proposed improvements are highlighted within the **East Gateway** section. No other short term safety improvements were identified within the North Gateway.



TRANSPORTATION AND MOBILITY FRAMEWORK



The North Gateway is a major thoroughfare within the community. The corridor is part of Hennepin County's County Highway 101 corridor and serves a greater regional mobility need. The roadway includes two-travel lanes in each direction today with recorded traffic volumes over 15,000 vehicles per day. There are limited dedicated turn lanes throughout this segment which can cause delays or near misses as drivers attempt turning movements. There are narrow sidewalks along the segment that are adjacent to the roadway curb. The following table details the existing design elements within the North Gateway.

Future traffic volumes were explored, accounting for both identified redevelopment opportunities and assumed traffic growth. 2040 traffic volumes are forecasted to grow to 22,000 vehicles per day, an increase of 32 percent over 19 years. Both the existing volumes and forecasted volumes were used to analyze recommendations for this segment.

| Roadway | # of Lanes | Speed Limit | Center Turn Lane/Median | On-street parking | Multi-modal Infrastructure | Sidewalks or trail | Traffic volume (Year) |
|-------------------------|------------|-------------|-------------------------|-------------------|----------------------------|-------------------------|-----------------------|
| Existing Central Avenue | 4 | 35 mph | None | None | N/A | Sidewalks on both sides | 16,700 (2019) |

PART C: FRAMEWORK & CORRIDOR STRATEGIES

Design Recommendations

Opportunities for the North Gateway focused on maintaining overall mobility and enhanced multimodal infrastructure. The design modifications present a new cross section where one southbound travel lane would be converted into a left turn lane or median and both northbound travel lanes would be maintained. The narrowing of lanes would also create the opportunity to add a grassed buffer between the roadway and sidewalk on either side. Existing overhead transmission lines are present along the eastern side of Central Avenue and will need to be factored into the overall design. Burial of the lines, while a larger undertaking would open multiple options for transportation infrastructure within the right of way.

| Roadway | # of Lanes | Speed Limit | Center Turn Lane/Median | On-street parking | Multi-modal Infrastructure | Sidewalks or trail |
|----------------------|-----------------------------------|-------------|-------------------------|-------------------|----------------------------|-------------------------|
| Proposed Central Ave | 3 thru lanes & 1 center left-turn | 30 mph | Center Turn Lane | None | N/A | Sidewalks on both sides |

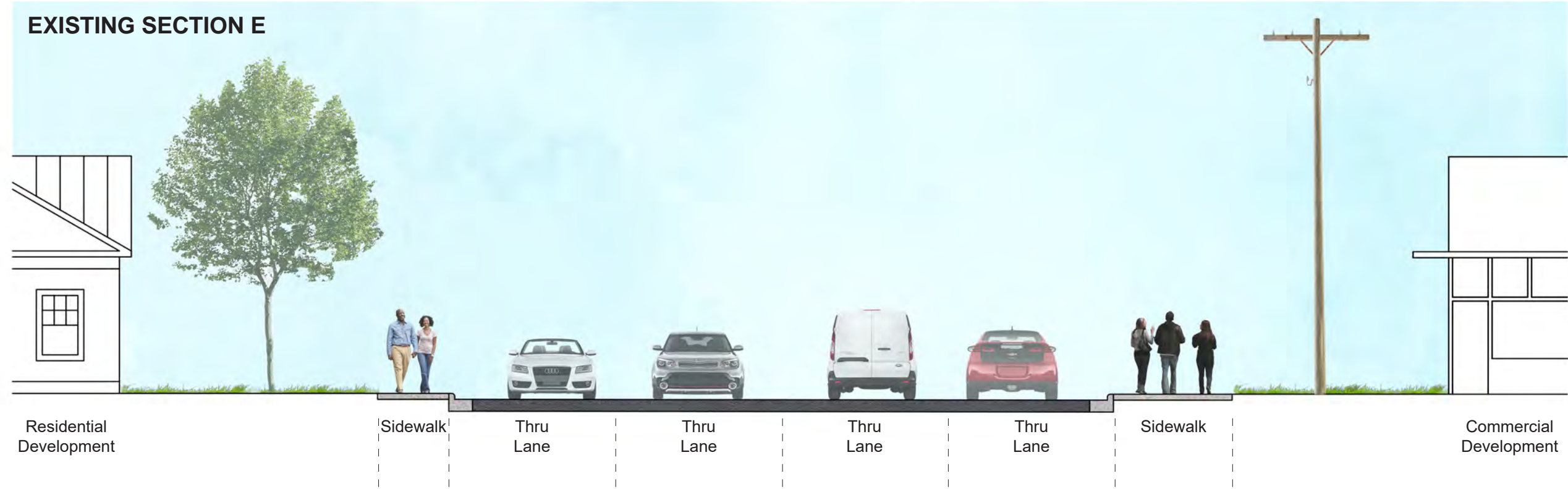
Spot Improvements

In addition to the overall design updates, location specific strategies were also identified to support the corridor vision, mobility, and sense of place. The specific identified improvements include:

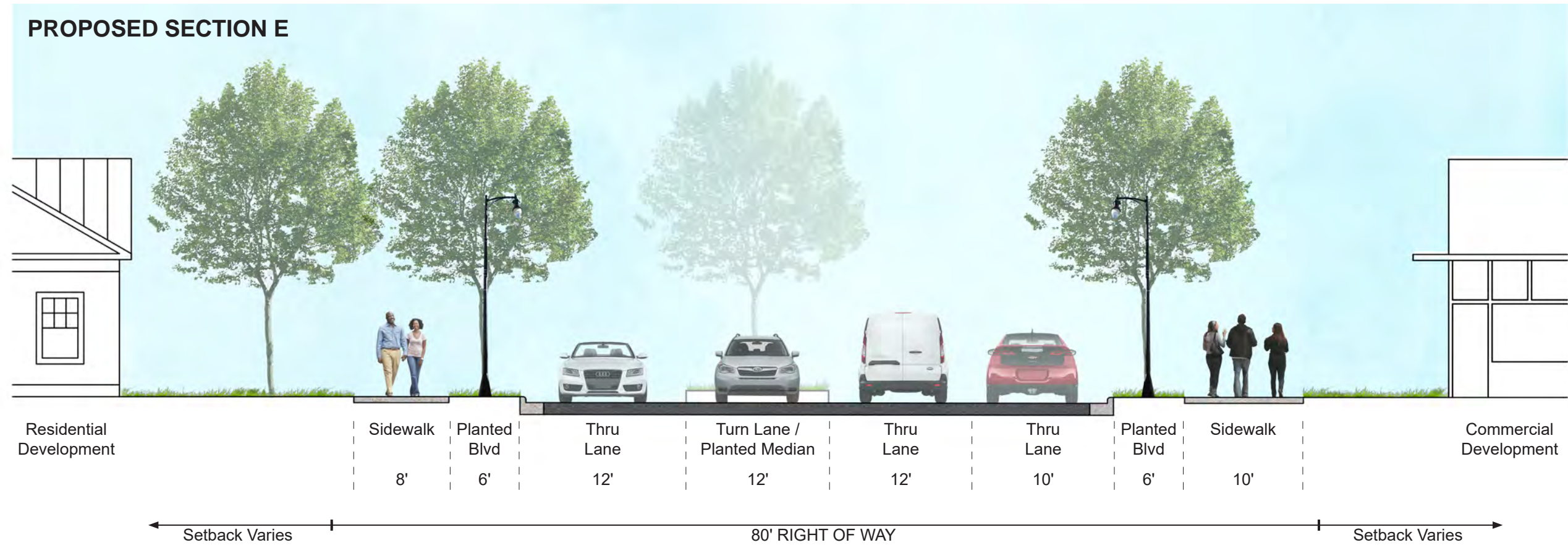
| | |
|--|--|
| Central Avenue and Wayzata Boulevard | <p>Add Curb Extensions: Add curb extensions to pedestrian refuge islands to improve visibility of pedestrians and to provide a buffer between pedestrians and turning traffic.</p> <p>Signal Timing Upgrade: Incorporate leading pedestrian intervals and increased crossing times into signals to increase pedestrian visibility and to allow more time for crossing.</p> |
| Midway Crossings near Colonial Square | <p>Midway Crossing: Incorporate midway crossing near Colonial Square to expand mobility options and pedestrian access to local amenities.</p> |
| Close Southern Colonial Square Entrance | <p>Omit Entrance / Exit: Omit the entrance and exit to the shopping center near the intersection of Wayzata Blvd and Central Ave to mitigate circulation conflicts with northbound traffic.</p> |
| Midway Crossing near Wayzata Medical Center | <p>Midway Crossing: Incorporate midway crossing near Wayzata Medical Center to expand mobility options and pedestrian access to local amenities.</p> |

Wayzata North Gateway

EXISTING SECTION E



PROPOSED SECTION E



PART C: FRAMEWORK & CORRIDOR STRATEGIES

Bicycle and Pedestrian Infrastructure

The proposed roadway improvements include upgrades to the existing sidewalks to increase the width and incorporate a buffer between the roadway and sidewalk, increasing the feeling of safety for traveling pedestrians.

PUBLIC REALM IMPROVEMENTS FRAMEWORK

The public realm varies throughout the North Gateway with a combination of residential and commercial frontages. Commercial developments along the corridor create active spaces for the public to interact with, while residential areas limit the public realm space with fences and vegetation. The public realm considerations for each of the public realm zones are identified within the table below.

| Zone | North Gateway Considerations | North Gateway Recommendations |
|------------------------|---|--|
| Structure Zone | The Structure Zone includes primarily commercial structures that must align with the community design standards which creates a unified sense of place through redevelopment. The setback guidance for this gateway would prioritize a 15' setback with public uses and landscaping, while also allowing a larger 40' setback to accommodate one row of parking with landscaped buffer. | <ul style="list-style-type: none"> • Consistent material and standards • Consistent setbacks |
| Frontage Zone | <p>The Frontage Zone is an opportunity for a combination of public and private investments to solidify the sense of place, providing gathering spaces, and provide places of refuge and rest.</p> <p>The Frontage Zone on the western segment of the corridor includes many private residential front yards, so activation of this space will be limited, especially with the presence of private fences.</p> | <ul style="list-style-type: none"> • Accessible connections • Corridor lighting • Refuge and rest areas • Gathering spaces • Greenspaces and vegetation |
| Pedestrian Zone | The Pedestrian Zone will be improved through this segment will the infill of sidewalk gaps to complete the overall network. These improvements also create the opportunity to support aesthetic improvements and are they key location for wayfinding signage. | <ul style="list-style-type: none"> • Public art • Wayfinding |
| Buffer Zone | The Buffer Zone would be established with the implementation of the transportation and mobility recommendations for the North Gateway. | <ul style="list-style-type: none"> • Corridor lighting • Wayfinding • Greenspaces and vegetation |

PART C: FRAMEWORK & CORRIDOR STRATEGIES

| Zone | North Gateway Considerations | North Gateway Recommendations |
|---------------------|---|--|
| Roadway Zone | The Roadway Zone is dedicated to the infrastructure that moves vehicles and bicyclists. This zone includes the roadway starting at the curb with the improvements described in the Transportation and Mobility Framework. | <ul style="list-style-type: none"> • Crossing Signage • Crosswalk improvements |

Gateway Areas

The North Gateway is the first experience travelers from the north have into the community. This creates opportunities for gateway signage to emphasize the arrival into Wayzata and establish the overall sense of place. There are multiple opportunities to incorporate gateway signage and infrastructure, including larger signage to the south of the Highway 12 interchange and banners or other elements in new median areas.

Public Art and Sense of Place

In addition to specific gateway improvements, other opportunities to introduce public art could be explored through the Pedestrian and Frontage Zones. At a minimum, the addition of planted medians introduces additional vegetation areas with opportunities for art installations.

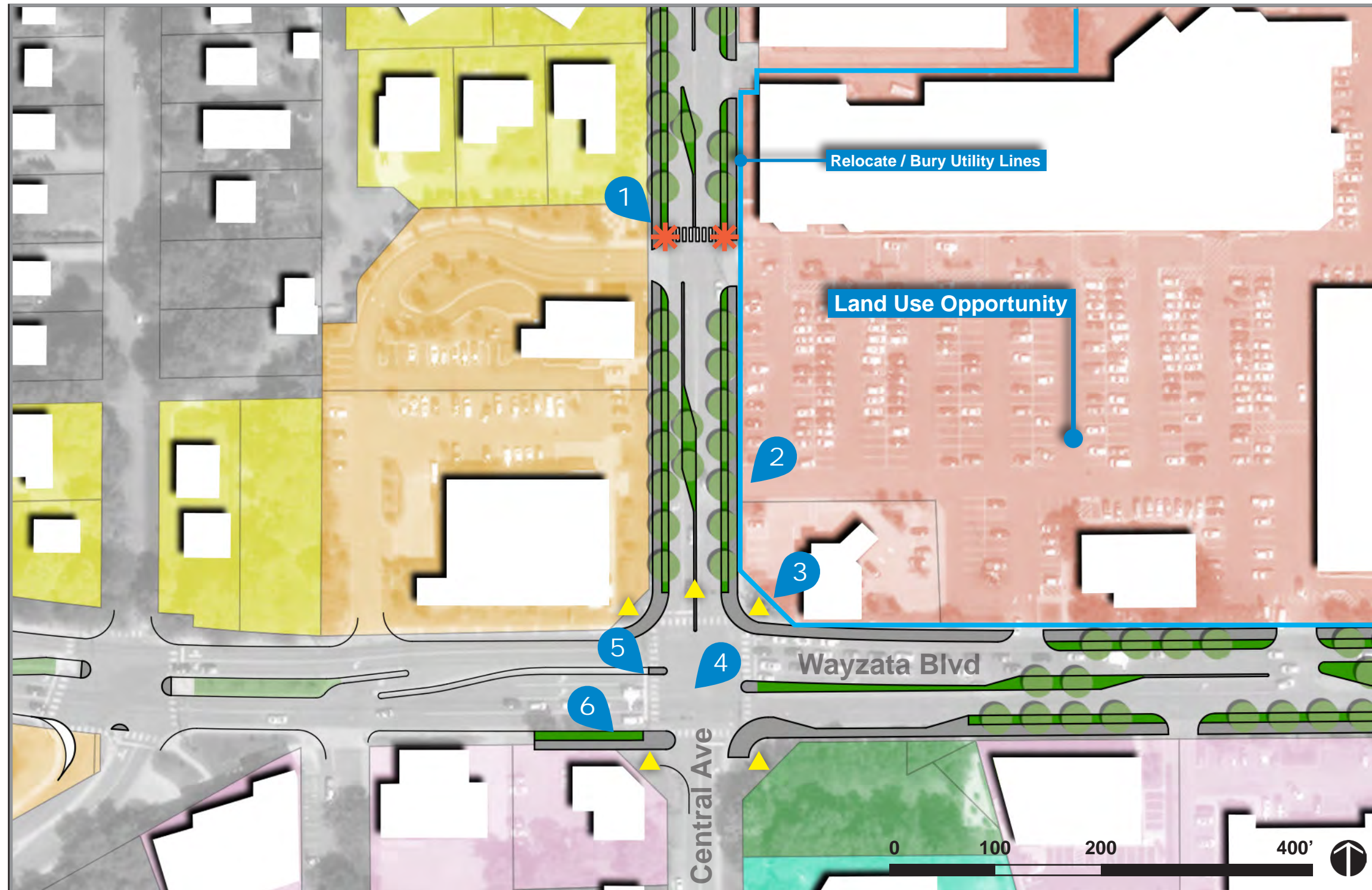
PART C: FRAMEWORK & CORRIDOR STRATEGIES

ILLUSTRATIVE PLAN

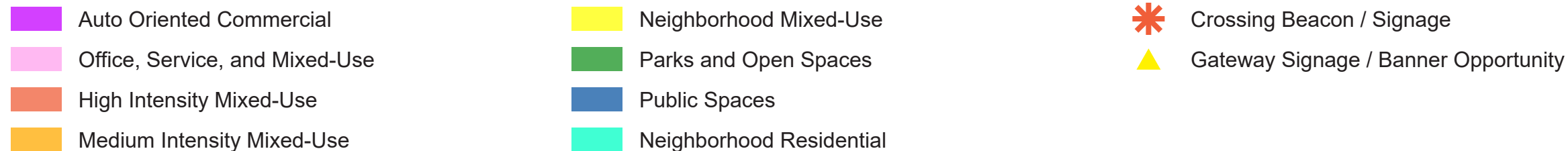
The following pages and rendering summarize the framework and strategies identified for the North Gateway. The rendering below represents a graphical illustration of the planning level design identified through this study. Specific design features will be refined through the design process, and modifications from the graphic below are anticipated.



Wayzata North Gateway



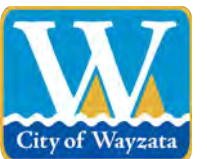
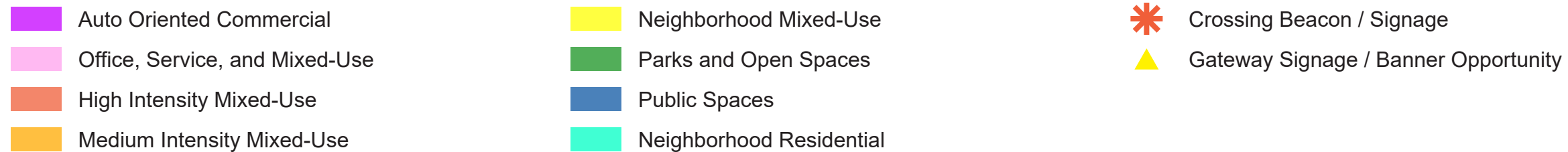
- 1 **Midway Crossing**
Incorporate midway crossing near Colonial Square to expand mobility options and pedestrian access to local amenities.
- 2 **Omit Entrance / Exit**
Omit the entrance and exit to Colonial Square near the intersection of Wayzata Blvd and Central Ave to mitigate circulation conflicts with northbound traffic.
- 3 **Gateway Opportunity**
Identify opportunities for gateway signage and banners at Wayzata Blvd and Central Ave.
- 4 **Close Sidewalk Gaps**
Close sidewalk gaps at southwest quadrant of Wayzata Blvd and Central Ave.
- 5 **Add Curb Extensions**
Add curb extensions to pedestrian refuge islands to improve visibility of pedestrians and to provide a buffer between pedestrians and turning traffic.
- 6 **Signal Timing Upgrade**
Incorporate leading pedestrian intervals and increased crossing times into signals to increase pedestrian visibility and to allow more time for crossing.



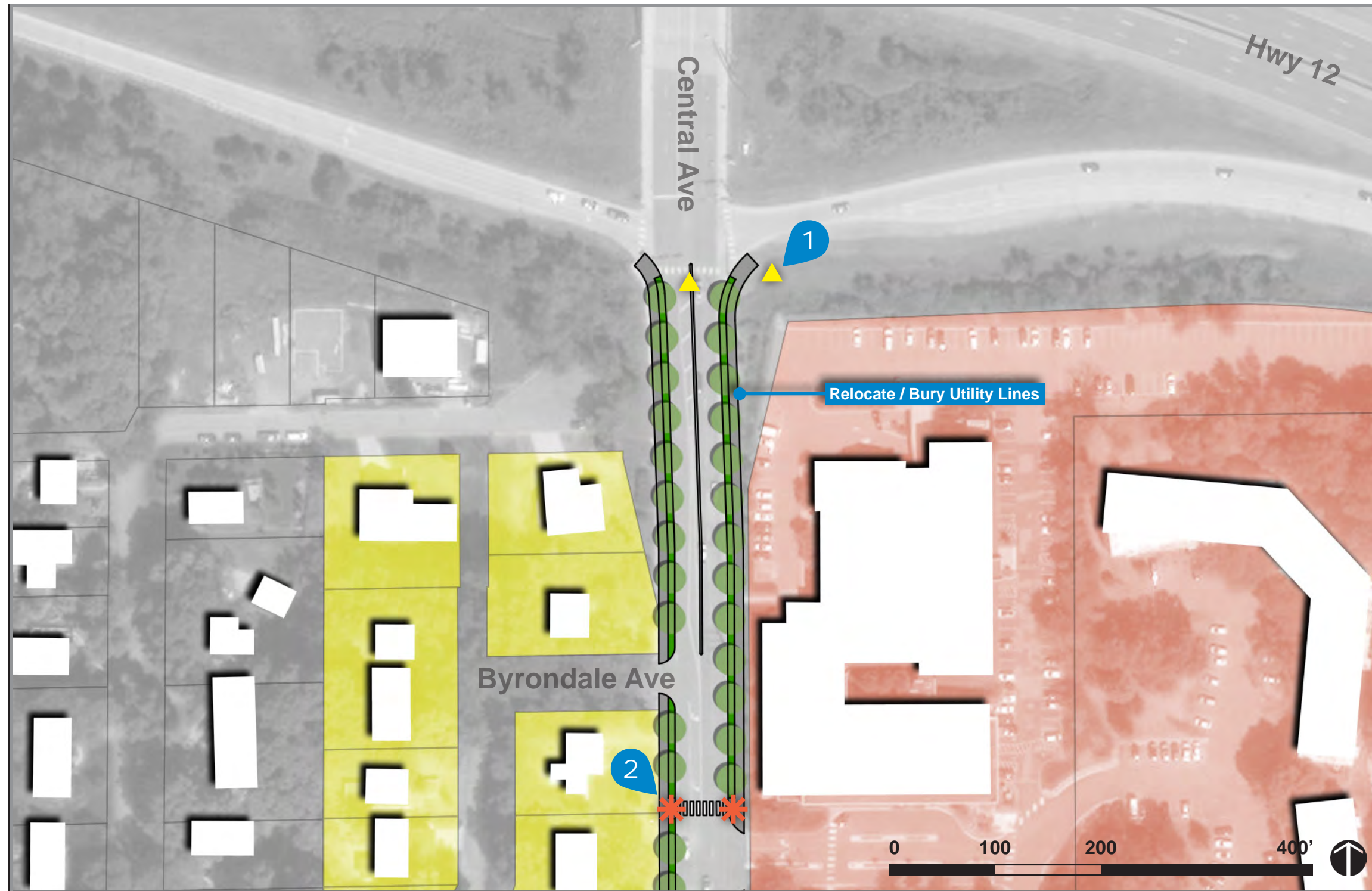
Wayzata North Gateway



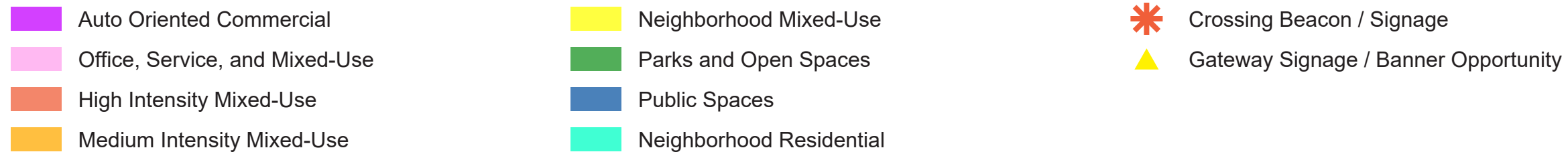
- 1 **Midway Crossing**
Incorporate midway crossings near Colonial Square and the Wayzata Medical Center to expand mobility options and pedestrian access to local amenities.
- 2 **Green Space Connection**
Identify future connections between green spaces.



Wayzata North Gateway



- 1 **Midway Crossing**
Incorporate midway crossing near the Wayzata Medical Center to expand mobility options and pedestrian access to local amenities.
- 2 **Gateway Opportunity**
Identify opportunities for gateway signage and banners at the North Gateway.



PART C: FRAMEWORK & CORRIDOR STRATEGIES

WAYZATA BOULEVARD TOWN CORE PLAN

The Town Core segment includes Wayzata Boulevard from Minnetonka Avenue to Superior Boulevard. This segment of the study area includes the most recent updates with the addition of the planted medians.

LAND USE AND DEVELOPMENT FRAMEWORK

There are several established uses within the Town Core area. Development opportunities are specifically focused where the conditions and context would like support public or private investments.



Land Use Districts

Four land use districts were identified within the Town Core area. In addition to the corridor-wide recommendations identified, the following characteristics should be considered as development is pursued.

| | |
|---------------------------------------|---|
| Medium Intensity Mixed-Use | The Medium Intensity Mixed-Use district identifies opportunities for larger scale redevelopment along the corridor, at a slightly smaller scale than the High Intensity areas. This district was specifically identified for the northwest quadrant of Wayzata Boulevard and Central to identify opportunities for redevelopment that introduces commercial and residential uses. Additionally, the current redevelopment of the former Wells Fargo property is classified within this district |
| Neighborhood Mixed-Use | The Neighborhood Mixed-Use district supports the maintenance of existing residential uses along the Town Core, with the opportunity to introduce neighborhood commercial uses as desired. Opportunities to include neighborhood mixed-use will occur as desired with property owners. |
| Office, Service, and Mixed-Use | Office, Service, and Mixed-Use creates opportunities to support the existing land uses along the northern and southern edge of Wayzata Boulevard with the opportunity to introduce residential uses through redevelopment. |
| Public Spaces | St Bartholomew of Wayzata is an established use that is anticipated to remain for some time. |

PART C: FRAMEWORK & CORRIDOR STRATEGIES

Land Use Opportunities

Three sites have been identified as potential land use opportunities. Simply, these are key areas for public and/or private exploration.

| Site | Address | Size | Description |
|--------------------|--|------------|--|
| Wayzata Home | 822 Wayzata Boulevard 105 Grand Avenue | 0.75 acres | The site currently houses active commercial uses with the eastern parcel dedicated to parking. Opportunities to explore the highest and best use for the property could be explored to identify redevelopment opportunities. The grade of the site may also present some development barriers. |
| Former Gas Station | 1022 Wayzata Boulevard 1042 Wayzata Boulevard | 0.80 acres | These sites include current and former commercial uses and could be key redevelopment opportunities based on their location along the corridor. Development barriers due to the former use as a gas station and potential soil contaminants create challenges for future uses and the overall development costs. |

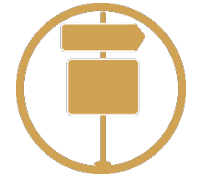


Former Gas Station

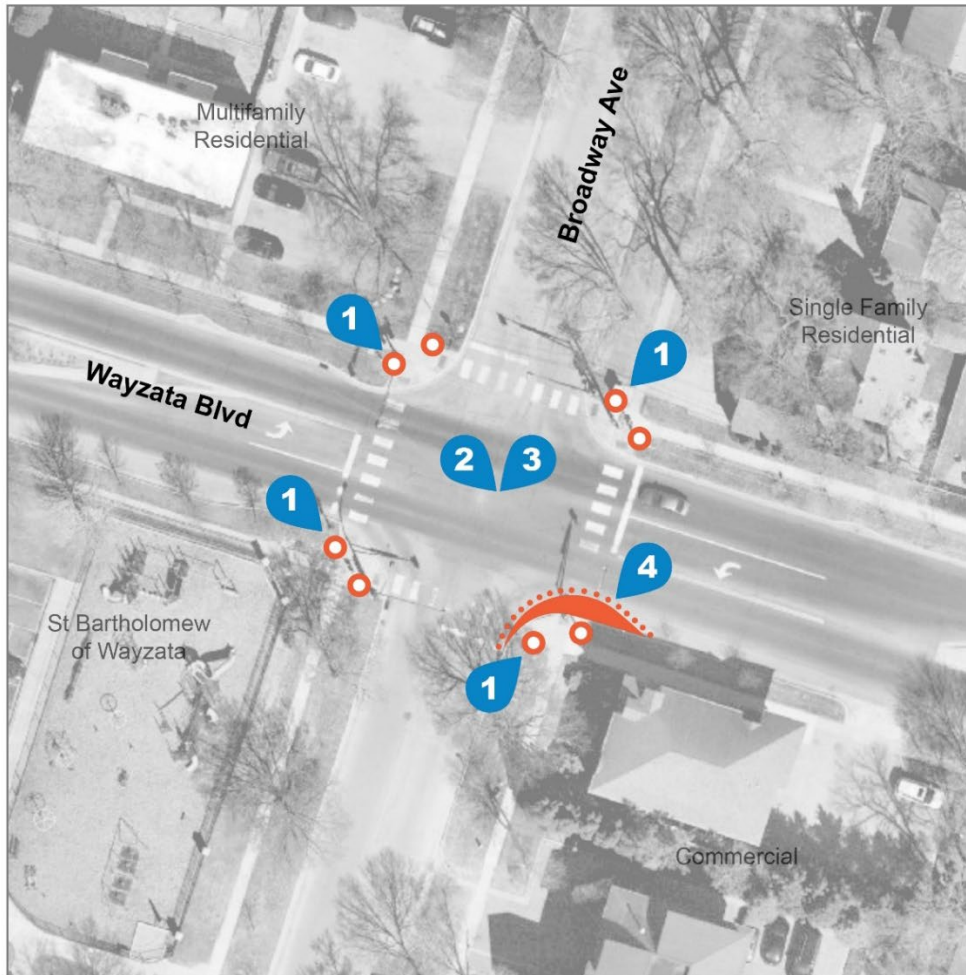
PART C: FRAMEWORK & CORRIDOR STRATEGIES

SAFETY IMPROVEMENTS FRAMEWORK

The Broadway Avenue and Wayzata Boulevard and the Superior Boulevard and Wayzata Boulevard intersections were identified for short term improvements based on past summaries crash history and the existing intersection design.



Broadway Avenue and Wayzata Boulevard



1 Move Crosswalk Buttons and Update Quadrants

Separate crosswalk buttons and update push button landings and ramps to meet ADA compliance.

2 Existing School Safety Improvement

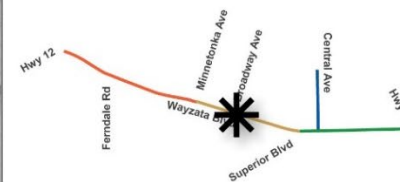
No right turn during peak morning and afternoon school traffic is an existing improvement.

3 Signal Timing Upgrade

Incorporate leading pedestrian intervals and increased crossing times into signals to increase pedestrian visibility and to allow more time for crossing.

4 Add Curb Extension

Add a curb extension to the SE quadrant to improve visibility between pedestrians and oncoming traffic and to make crossings safer for pedestrians.



PART C: FRAMEWORK & CORRIDOR STRATEGIES

Superior Boulevard and Wayzata Boulevard



1 Add Curb Extensions

Add curb extensions to pedestrian refuge islands to improve visibility of pedestrians and to provide a buffer between pedestrians and turning traffic.

2 Signal Timing Upgrade

Incorporate leading pedestrian intervals and increased crossing times into signals to increase pedestrian visibility and to allow more time for crossing.

3 Add Truck Apron

Add a truck apron to the SW quadrant to lower turning speeds for vehicles turning right onto Superior Blvd by providing turning radii for both automobiles and larger freight vehicles.

4 No Turn on Red

Convert the NB right turn onto Wayzata Blvd from a free right to a no turn on red to lower vehicle speeds and to increase pedestrian safety.



PART C: FRAMEWORK & CORRIDOR STRATEGIES

TRANSPORTATION AND MOBILITY FRAMEWORK



The Town Core experienced the most recent improvements of all segments of the study area. The construction of the planted median through this segment provided a drastic shift for the overall experience along Wayzata Boulevard and the segment historically supports less than 10,000 vehicles per day across two-lanes of travel. The wide shoulders throughout this segment also support on-street parking opportunities.

Future traffic volumes were explored, accounting for both identified redevelopment opportunities and assumed traffic growth. 2040 traffic volumes are forecasted to grow to 17,000 vehicles per day, an increase of 16 percent over 19 years. Both the existing volumes and forecasted volumes were used to analyze recommendations for this segment.

| Roadway | # of Lanes | Speed Limit | Center Turn Lane/Median | On-street parking | Multi-modal Infrastructure | Sidewalks or trail | Traffic volume (Year) |
|-----------------------|------------|-------------|-------------------------|-------------------|----------------------------|------------------------------------|-----------------------|
| Existing Wayzata Blvd | 2 | 35 mph | Median | Yes | Bus stops with Benches | Sidewalks with gaps on either side | 14,700 (2019) |

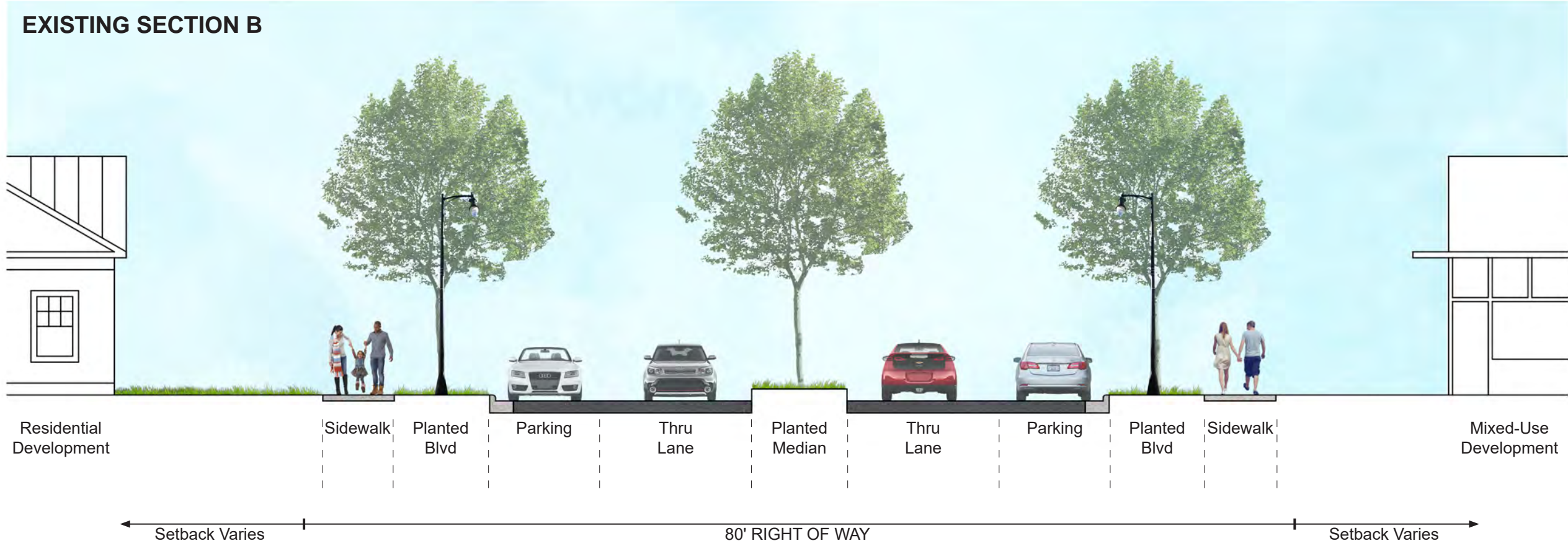
Design Recommendations

Opportunities for the Town Core generally maintained the existing design with opportunities to close sidewalk gaps and crossings.

| Roadway | # of Lanes | Speed Limit | Center Turn Lane/Median | On-street parking | Multi-modal Infrastructure | Sidewalks or trail |
|-----------------------|------------|-------------|-------------------------|-------------------|----------------------------|--------------------|
| Proposed Wayzata Blvd | 2 | 30 mph | Median | Yes (non-striped) | Bus stops with Benches | Sidewalk |

Wayzata Town Core

EXISTING SECTION B



PART C: FRAMEWORK & CORRIDOR STRATEGIES

Spot Improvements

In addition to the overall design updates, location specific strategies were also identified to support the corridor vision, mobility, and sense of place. The specific identified improvements include:

| | |
|--|--|
| Walker Avenue and Wayzata Boulevard | Add Curb Extensions: Add curb extensions to the Walker Ave intersection quadrants to improve visibility between pedestrians and oncoming traffic and to make crossings safer for pedestrians. |
| Broadway Avenue and Wayzata Boulevard | Add Curb Extensions: Add curb extensions to the Broadway Blvd intersection quadrants to improve visibility between pedestrians and oncoming traffic and to make crossings safer for pedestrians. |
| Chicago Avenue and Wayzata Boulevard | Add Curb Extensions: Add curb extensions to the Chicago Avenue intersection quadrants to improve visibility between pedestrians and oncoming traffic and to make crossings safer for pedestrians. |
| Benton Avenue/Superior Boulevard and Wayzata Boulevard | <p>Add Truck Apron: Add a truck apron to the SW quadrant to lower turning speeds for vehicles turning right onto Superior Blvd by providing turning radii for both automobiles and larger freight vehicles.</p> <p>Add Curb Extensions: Add curb extensions to pedestrian refuge islands to improve visibility of pedestrians and to provide a buffer between pedestrians and turning traffic.</p> <p>Signal Timing Upgrade: Incorporate leading pedestrian intervals and increased crossing times into signals to increase pedestrian visibility and to allow more time for crossing.</p> <p>No Turn on Red: Convert the northbound right turn onto Wayzata Blvd from a free right to a no turn on red to lower vehicle speeds and to increase pedestrian safety.</p> |

PUBLIC REALM IMPROVEMENTS FRAMEWORK

The public realm is relatively consistent through the Town Core with primarily adjacent residential uses. The presence of residential uses limits the ability to activate the public realm to the Pedestrian and Buffer Zones through this segment. The public realm considerations for each of the public realm zones are identified within the table below.

PART C: FRAMEWORK & CORRIDOR STRATEGIES

| Zone | West Gateway Considerations | West Gateway Recommendations |
|------------------------|--|---|
| Structure Zone | The Structure Zone varies greatly throughout this segment from smaller residential uses to large public uses. The setback guidance for this gateway includes areas within both the Boulevard District and Bluff District. Areas within the Boulevard District will prioritize a 15' setback with public uses and landscaping, while also allowing a larger 40' setback to accommodate one row of parking with landscaped buffer. Areas within the Bluff District will maintain the existing 20' setback that has shaped the current development. | <ul style="list-style-type: none"> • Consistent material and standards |
| Frontage Zone | The Frontage Zone for single family residential uses through this segment is not primed for activation and should be reserved for private uses. | <ul style="list-style-type: none"> • Corridor lighting • Greenspaces and vegetation |
| Pedestrian Zone | The Pedestrian Zone will be improved through this segment will the infill of sidewalk gaps to complete the overall network. These improvements also create the opportunity to support aesthetic improvements and are they key location for wayfinding signage. | <ul style="list-style-type: none"> • Public art • Wayfinding • Transit shelters and refuge |
| Buffer Zone | The Buffer Zone should be maintained throughout this segment to provide a greenspace buffer between the trail/sidewalk and the roadway edge. This zone may vary in width throughout the section | <ul style="list-style-type: none"> • Corridor lighting • Wayfinding • Greenspaces and vegetation • Crossing Signage |
| Roadway Zone | The Roadway Zone is dedicated to the infrastructure that moves vehicles and bicyclists. This zone includes the roadway starting at the curb with the improvements described in the Transportation and Mobility Framework | <ul style="list-style-type: none"> • Planted Median • Crosswalk improvements |

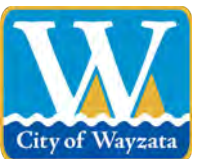
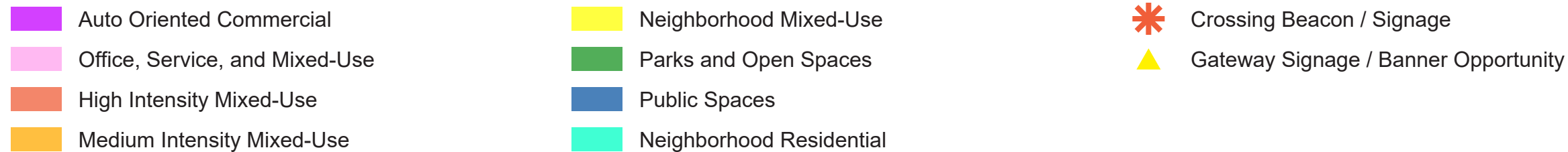
ILLUSTRATIVE PLAN

The following pages summarize the framework and strategies identified for the Town Center.

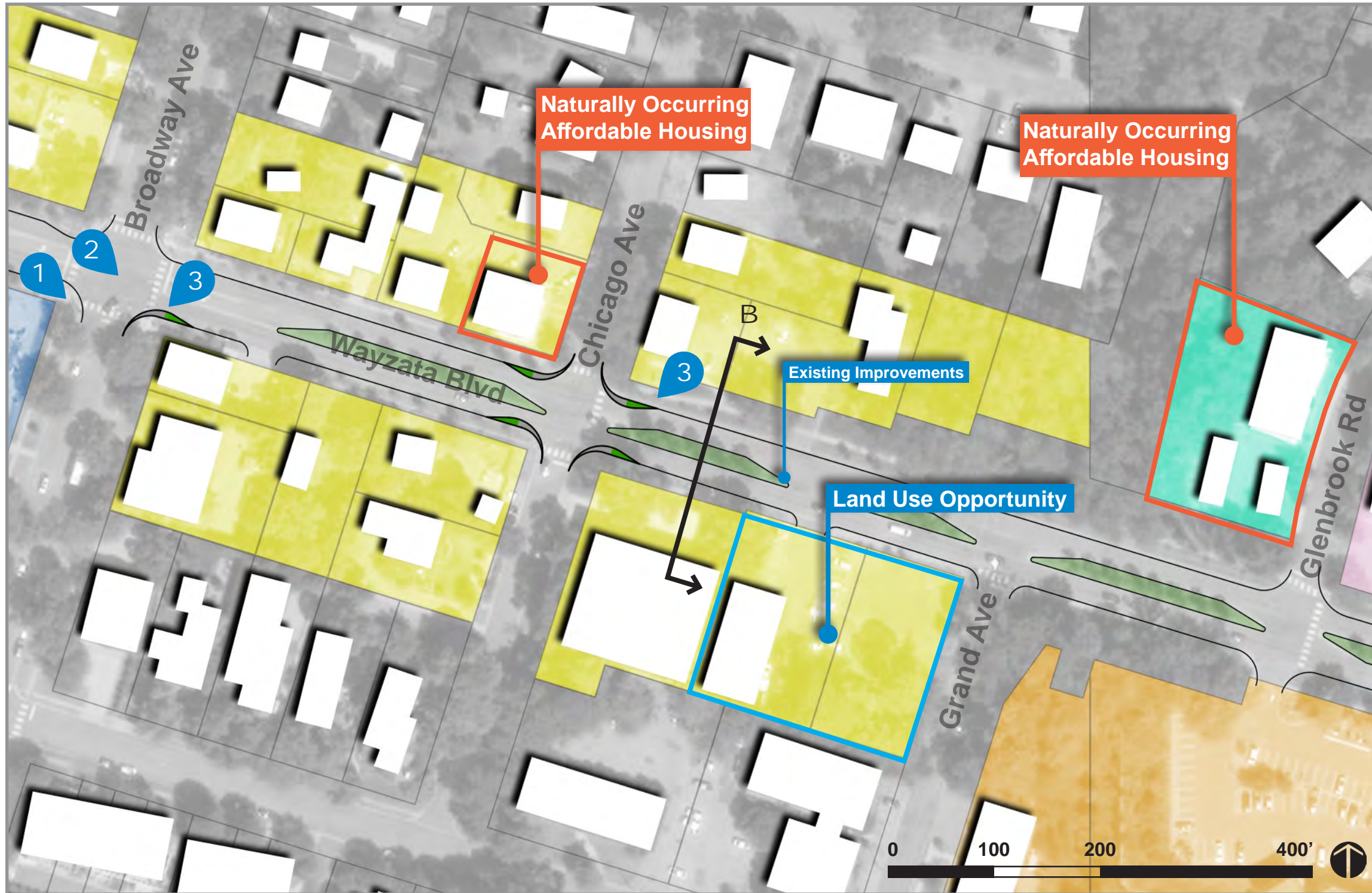
Wayzata Town Core



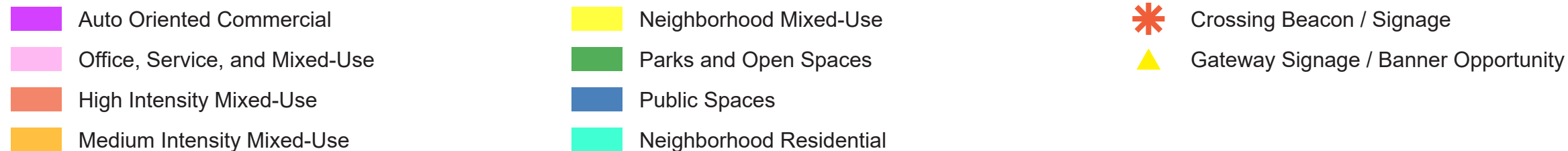
- 1 **Close Sidewalk Gap**
Close the sidewalk gap at the southwest quadrant of Wayzata Blvd and Minnetonka Ave.
- 2 **Add Curb Extensions**
Add curb extensions to the Walker Ave and Broadway Ave intersection quadrants to improve visibility between pedestrians and oncoming traffic and to make crossings safer for pedestrians.
- 3 **Signal Timing Upgrade**
Incorporate leading pedestrian intervals and increased crossing times into signals to increase pedestrian visibility and to allow more time for crossing.
- 4 **Move Crosswalk Buttons and Update Quadrants**
Separate crosswalk buttons and update push button landings and ramps to meet ADA compliance.



Wayzata Town Core



- 1 **Move Crosswalk Buttons and Update Quadrants**
Separate crosswalk buttons and update push button landings and ramps to meet ADA compliance.
- 2 **Signal Timing Upgrade**
Incorporate leading pedestrian intervals and increased crossing times into signals to increase pedestrian visibility and to allow more time for crossing.
- 3 **Add Curb Extensions**
Add a curb extension to the Broadway Ave and Chicago Ave intersection quadrants to improve visibility between pedestrians and oncoming traffic and to make crossings safer for pedestrians.



Wayzata Town Core



- 1 **Add Truck Apron**
Add a truck apron to the SW quadrant to lower turning speeds for vehicles turning right onto Superior Blvd by providing turning radii for both automobiles and larger freight vehicles.
- 2 **Add Curb Extensions**
Add curb extensions to pedestrian refuge islands to improve visibility of pedestrians and to provide a buffer between pedestrians and turning traffic.
- 3 **Signal Timing Upgrade**
Incorporate leading pedestrian intervals and increased crossing times into signals to increase pedestrian visibility and to allow more time for crossing.
- 4 **No Turn on Red**
Convert the NB right turn onto Wayzata Blvd from a free right to a no turn on red to lower vehicle speeds and to increase pedestrian safety.
- 5 **Close Sidewalk Gaps**
Close sidewalk gaps at southwest quadrant of Wayzata Blvd and Central Ave.
- 6 **Gateway Opportunity**
Identify opportunities for gateway signage and banners at Wayzata Blvd and Central Ave.

- Auto Oriented Commercial
- Neighborhood Mixed-Use
- Office, Service, and Mixed-Use
- Parks and Open Spaces
- High Intensity Mixed-Use
- Public Spaces
- Medium Intensity Mixed-Use
- Neighborhood Residential

- Crossing Beacon / Signage
- Gateway Signage / Banner Opportunity



PART C: FRAMEWORK & CORRIDOR STRATEGIES

WAYZATA WEST GATEWAY PLAN

The West Gateway includes the segment of Wayzata Boulevard from the bridge over Highway 12 to the intersection with Minnetonka Avenue. Today, this segment includes a wide roadway with one travel lane in each direction and gaps within the sidewalk network. There are no specific bicycle infrastructure elements along Wayzata Boulevard, but Ferndale Road and Barry Avenue have dedicated infrastructure, shared lane/sharrow and dedicated bike lane, respectively. From a development perspective, this segment includes a variety of residential uses – from apartments to larger lot single family homes. Wayzata West Middle School and Klapprich Park are key public amenities destinations, drawing a number of trips on a daily basis. Finally, an existing Metro Transit park-and-ride facility is in this segment. The site serves as a stop for transit service and driver amenities, along with its role as a park-and-ride.

LAND USE AND DEVELOPMENT FRAMEWORK

There are several established uses within the West Gateway area. Development opportunities are specifically focused where the conditions and context would like support public or private investments.



Land Use Districts

Four land use districts were identified within the West Gateway area. In addition to the corridor-wide recommendations identified, the following characteristics should be considered as development is pursued.

| | |
|-----------------------------------|--|
| Neighborhood Residential | This district represents a variety of residential uses, from single to multi-family. Support for general maintenance activities of existing uses. |
| Public Spaces | The Middle School is an established use that is anticipated to remain for some time. This site would provide unique opportunities for development that aligns with the Medium Intensity category. |
| Parks and Open Spaces | Klapprich Park is a beloved amenity within the community. Continued preservation and enhancement of the park will support community desires. |
| Medium Intensity Mixed-Use | This category represents the opportunities for reinvestment within the segment. Future development should align with the requirements but should also respond to the context of the area. For example, viable commercial uses may include more neighborhood commercial (e.g., coffee shop, office, etc.) that support surrounding residential uses and are not dependent on by drive by customers. |

PART C: FRAMEWORK & CORRIDOR STRATEGIES

Land Use Opportunities

Two sites have been identified for potential land use opportunities. Simply, these are key areas for public and/or private exploration.

| Site | Address | Size | Description |
|-----------------------------|---|-----------|---|
| Wayzata Medical Spa | 317 Wayzata Boulevard | 1.2 acres | The proximity to Klapprich Park and adjacent mixed use development area opens opportunities for alternative use and investment. Investments could include mixed-use redevelopment or expansion of the park. Future uses could include the redevelopment of a mixed-use building with ground level public uses and upper floor commercial or expansion of the park for additional green space and public uses (i.e., warming house). |
| Metro Transit Park-and-Ride | 320 Wayzata Boulevard 305 Rice Street E 322 Wayzata Boulevard | 2.7 acres | Modification of the existing Park-and-Ride to a higher and better use has been discussed for a number of years due to low use and transit ridership. This plan identifies the site as a key opportunity for redevelopment and an important example for the overall study. |

Connections

Connections between the Middle School and Klapprich Park are key considerations for connecting the community to these amenities. Visual and physical connections exist today and create opportunities for enhancement.

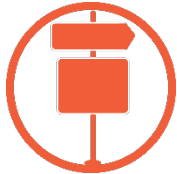


Wayzata Boulevard looking west toward Klapprich Park

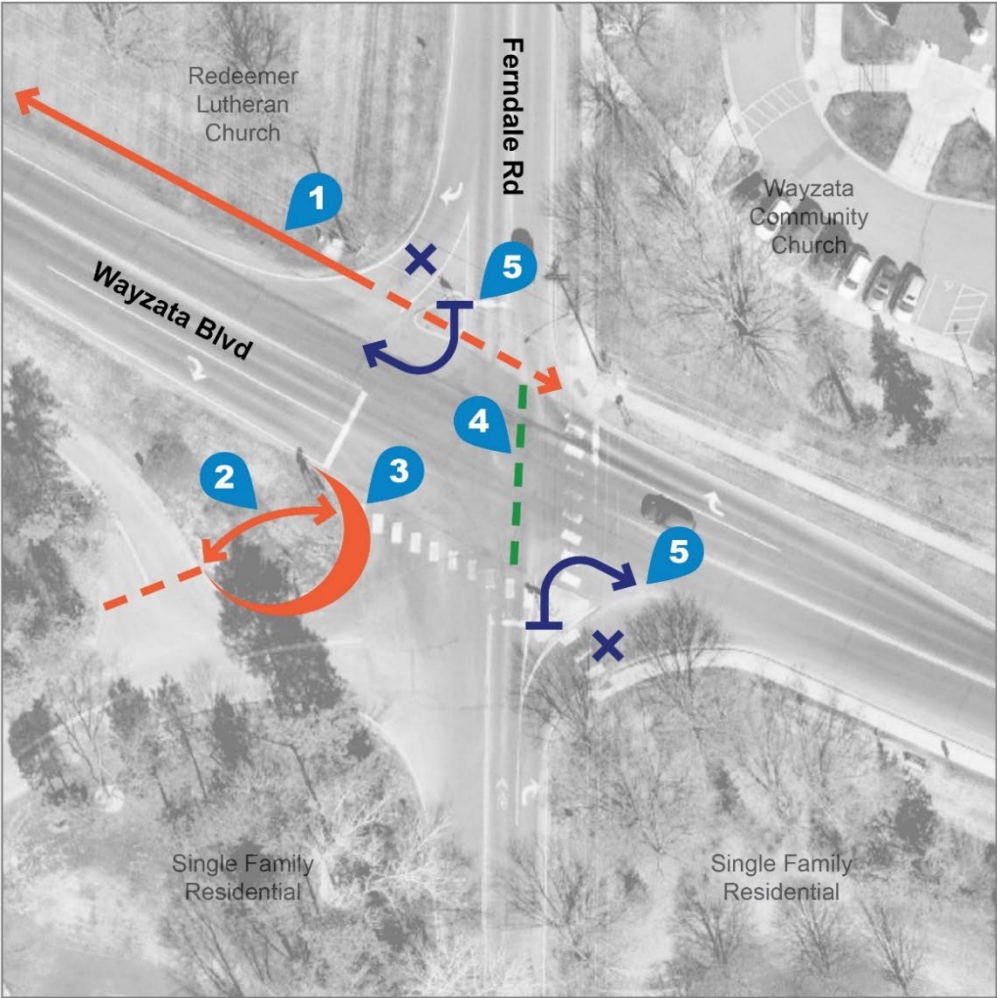
PART C: FRAMEWORK & CORRIDOR STRATEGIES

SAFETY IMPROVEMENTS FRAMEWORK

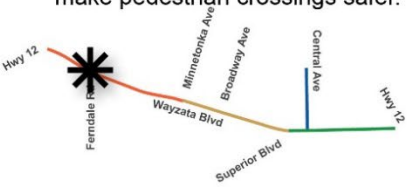
The Ferndale Road and Barry Avenue intersections with Wayzata Boulevard were identified for short term improvements based on past summaries crash history and the existing intersection design.



Ferndale Road and Wayzata Boulevard

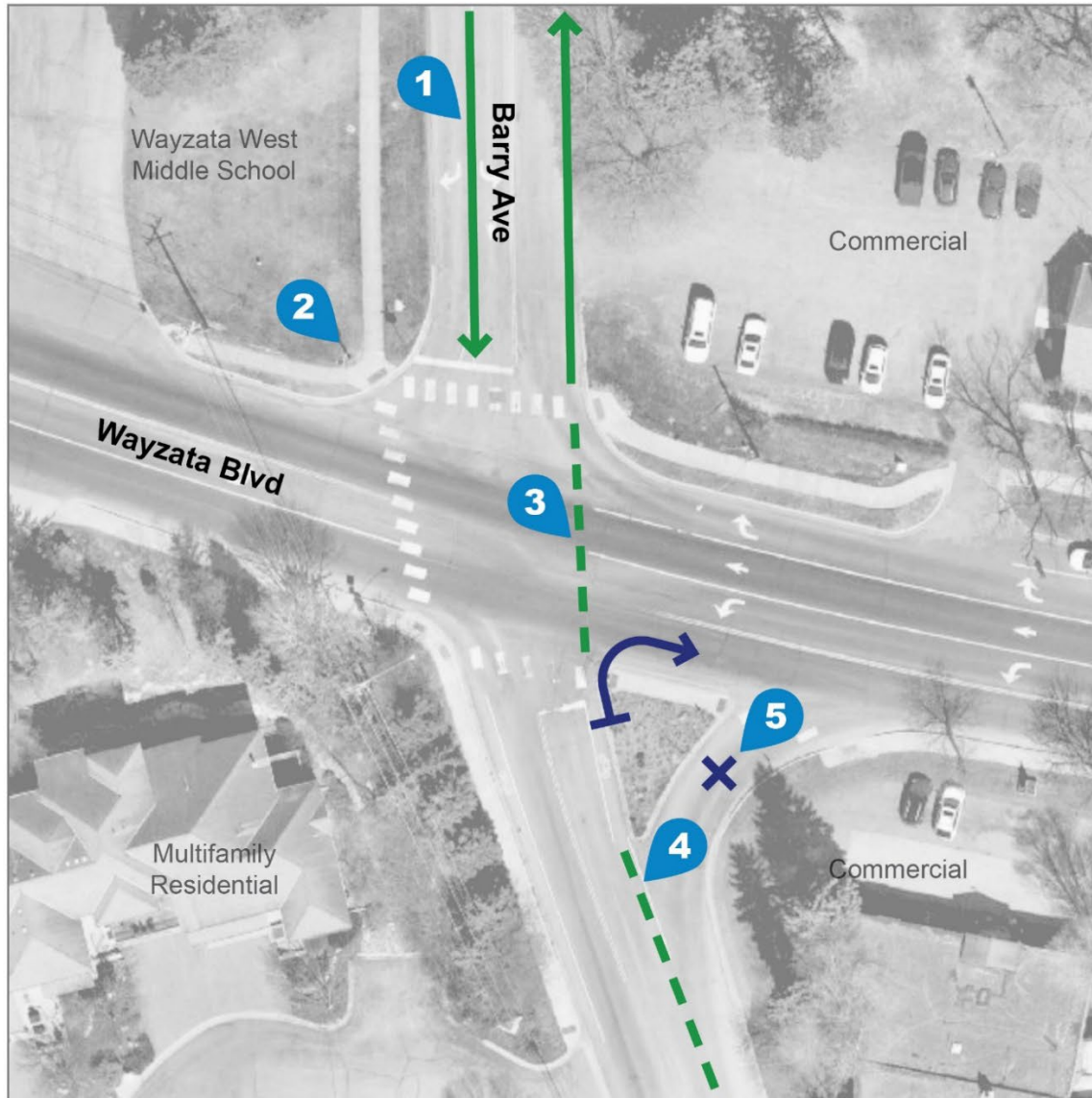


- 1 Add Crosswalk and Extend Sidewalk**
Add a crosswalk across Ferndale Rd and extend the sidewalk on the north side of Wayzata Blvd to build pedestrian connections with Redeemer Lutheran Church and Meridian Manor.
- 2 Improve Pedestrian Connection**
Improve the pedestrian connection across Ferndale Rd and into the neighborhood by adding a sidewalk and crosswalk.
- 3 Add Curb Extension**
Add a curb extension to the SW quadrant to improve visibility and to reduce the crossing distance for pedestrians.
- 4 Add Bike Crosswalk**
Add a bike crosswalk to
- 5 Omit Channelized Turns**
Omit channelized right turns to lower vehicle speeds and to make pedestrian crossings safer.

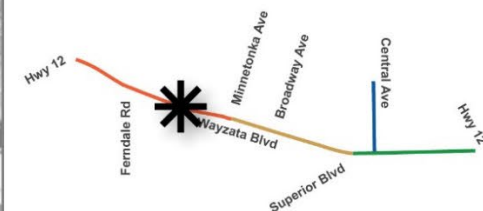


PART C: FRAMEWORK & CORRIDOR STRATEGIES

Barry Avenue and Wayzata Boulevard



- 1 Continue Bike Lanes**
Continue the bike lanes north of Wayzata Blvd to improve safety and connections for cyclists.
- 2 Existing RRFB (Rectangular Rapid Flashing Beacon)**
- 3 Add Bike Crosswalk**
Add a bike crosswalk to communicate that bike facilities continue across Wayzata Blvd.
- 4 Add Bike Lane Striping**
Add bike lane striping at the channelized right turn onto Wayzata Blvd to communicate to cyclists and drivers about the bike lane on Barry Ave.
- 5 Omit Channelized Turn**
Omit channelized right turns to lower vehicle speeds and to make pedestrian crossings safer.



PART C: FRAMEWORK & CORRIDOR STRATEGIES

TRANSPORTATION AND MOBILITY FRAMEWORK



The West Gateway area historically supports less than 10,000 vehicles per day across two-lanes of travel. The wide travel lanes with multiple turn lanes allow for efficient movement, but also present barriers for multimodal movement (e.g., crossings and sidewalk gaps). Barry Avenue and Ferndale Road include dedicated bicycle infrastructure and open the opportunities for future connections. The following table details the existing design elements within the West Gateway.

Future traffic volumes were explored, accounting for both identified redevelopment opportunities and assumed traffic growth. 2040 traffic volumes are forecasted to grow to 12,300 vehicles per day, an increase of 23 percent over 19 years. Both the existing volumes and forecasted volumes were used to analyze recommendations for this segment.

| Roadway | # of Lanes | Speed Limit | Center Turn Lane/Median | On-street parking | Multi-modal Infrastructure | Sidewalks or trail | Traffic volume (Year) |
|-----------------------|------------|-------------|-------------------------|-------------------|----------------------------|------------------------------------|-----------------------|
| Existing Wayzata Blvd | 2 | 35 mph | None | None | Bus stops with Benches | Sidewalks with gaps on either side | 10,000 (2019) |

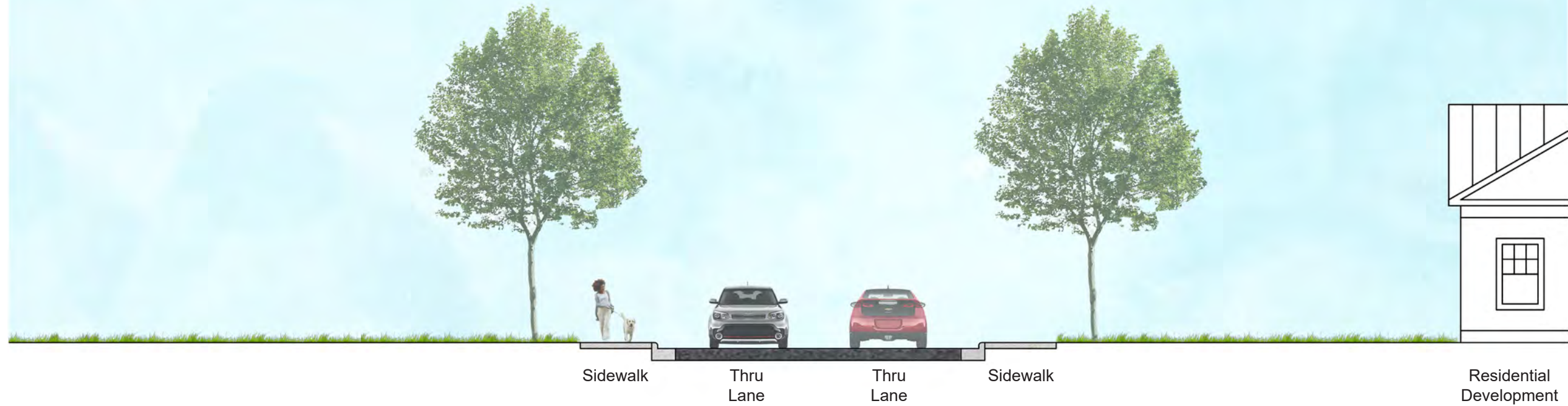
Design Recommendations

Opportunities for the West Gateway focused on maintaining overall mobility, extending the aesthetic and sense of place from the Town Core, and enhanced multimodal infrastructure. The design modifications maintain the overall travel lanes but include the introduction of a center median. Recommendations are also included to support bicycle and pedestrian connections. There are several sidewalk gaps that can be closed and the development of a shared use trail would support bicycle movement.

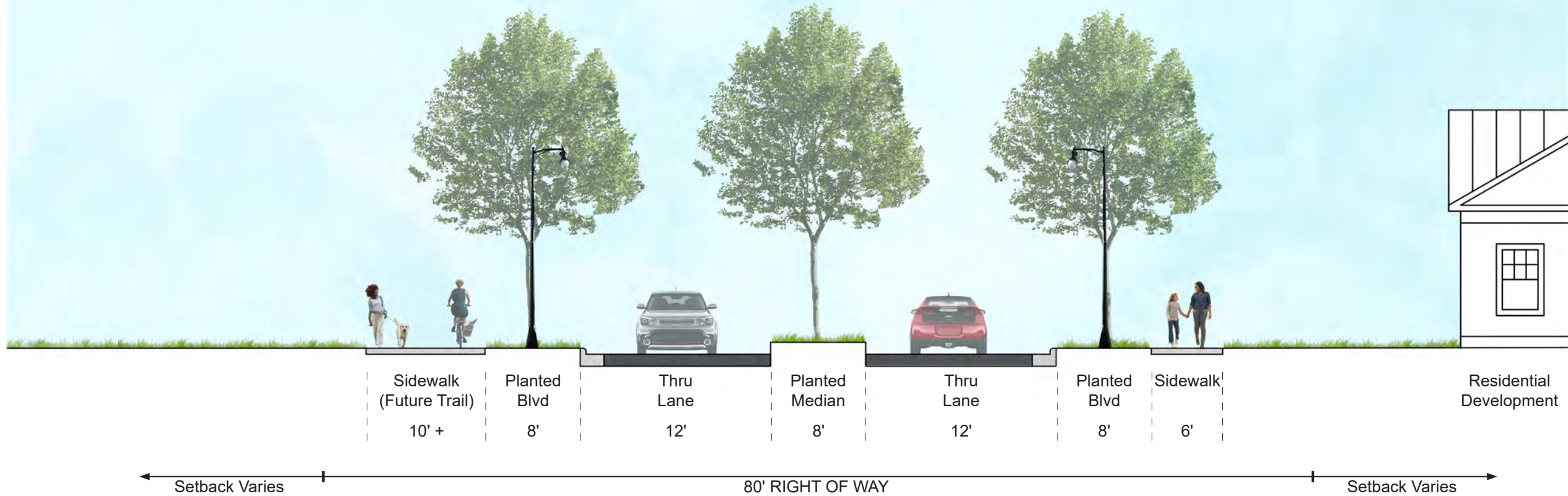
| Roadway | # of Lanes | Speed Limit | Center Turn Lane/Median | On-street parking | Multi-modal Infrastructure | Sidewalks or trail |
|-----------------------|------------|-------------|-------------------------|-------------------|----------------------------|--------------------|
| Proposed Wayzata Blvd | 2 | 30 mph | Median | None | Bus stops with Benches | Sidewalk and Trail |

Wayzata West Gateway

EXISTING SECTION A



PROPOSED SECTION A



PART C: FRAMEWORK & CORRIDOR STRATEGIES

Spot Improvements

In addition to the overall design updates, location specific strategies were also identified to support the corridor vision, mobility, and sense of place. The specific identified improvements include:

| | |
|--|--|
| Ferndale Road and Wayzata Boulevard | Omit Channelized Rights: Omit channelized right turns at Ferndale Rd and replace with mountable truck apron to lower vehicle speeds and to make pedestrian crossings safer. |
| Barry Avenue and Wayzata Boulevard | Add Bike Crosswalks: Add bike crosswalks to communicate that bike facilities continue across Wayzata Boulevard. Omit Channelized Rights: Omit channelized right turns at Ferndale Rd and replace with mountable truck apron to lower vehicle speeds and to make pedestrian crossings safer. |

Bicycle and Pedestrian Infrastructure

The existing bicycle infrastructure on Barry Avenue and Ferndale Road creates opportunities for enhancing multimodal infrastructure. Three Rivers Park District has also been exploring these roadways as part of a future regional trail. To support future designation and connection, a regional trail connection on the north side of Wayzata Boulevard as a shared use path expands access for the community.

PUBLIC REALM IMPROVEMENTS FRAMEWORK

The public realm is relatively consistent through the West Gateway, balancing residential and other frontages. The balance of residential frontages versus commercial/ or public frontages affect the ability to activate the public realm. The public realm considerations for each of the public realm zones are identified within the table below.

| Zone | West Gateway Considerations | West Gateway Recommendations |
|-----------------------|---|--|
| Structure Zone | The Structure Zone varies greatly throughout this segment from smaller residential uses to large public uses. The setback guidance for this gateway includes areas within the Bluff District, maintaining the existing 20' setback that has shaped the current development. | <ul style="list-style-type: none"> • Consistent material and standards |
| Frontage Zone | The Frontage Zone for multi-family residential, commercial, and other uses presents an opportunity for private investment in the public realm. Opportunities to create gathering spaces and comfort elements within these public frontages are encouraged. | <ul style="list-style-type: none"> • Accessible connections • Corridor lighting • Refuge and rest areas • Gathering spaces |

PART C: FRAMEWORK & CORRIDOR STRATEGIES

| Zone | West Gateway Considerations | West Gateway Recommendations |
|------------------------|---|---|
| Pedestrian Zone | <p>The Frontage Zone for single family residential uses through this segment is not primed for activation and should be reserved for private uses.</p> <p>The Pedestrian Zone will be improved through this segment with the infill of sidewalk gaps to complete the overall network. These improvements also create the opportunity to support aesthetic improvements and are the key location for wayfinding signage.</p> | <ul style="list-style-type: none"> • Greenspaces and vegetation • Public art • Wayfinding • Transit shelters and refuge |
| Buffer Zone | <p>The Buffer Zone should be maintained throughout this segment to provide a greenspace buffer between the trail/sidewalk and the roadway edge. This zone may vary in width throughout the section</p> | <ul style="list-style-type: none"> • Corridor lighting • Wayfinding • Greenspaces and vegetation • Crossing Signage |
| Roadway Zone | <p>The Roadway Zone is dedicated to the infrastructure that moves vehicles and bicyclists. This zone includes the roadway starting at the curb with the improvements described in the Transportation and Mobility Framework</p> | <ul style="list-style-type: none"> • Planted Median • Crosswalk improvements |

Gateway Areas

The West Gateway is the first experience travelers from the west have into the community. This creates opportunities for gateway signage to emphasize the arrival into Wayzata and establish the overall sense of place. There are multiple opportunities to incorporate gateway signage and infrastructure, including larger signage at the Ferndale Road intersection and banners or other elements in new median areas.

Public Art and Sense of Place

In addition to specific gateway improvements, other opportunities to introduce public art could be explored through the Pedestrian and Frontage Zones. At a minimum, the addition of planted medians introduces additional vegetation areas with opportunities for art installations. Adjacent public uses also create an opportunity to introduce public art installations or other investments.

PART C: FRAMEWORK & CORRIDOR STRATEGIES

ILLUSTRATIVE PLAN

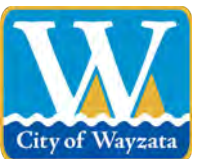
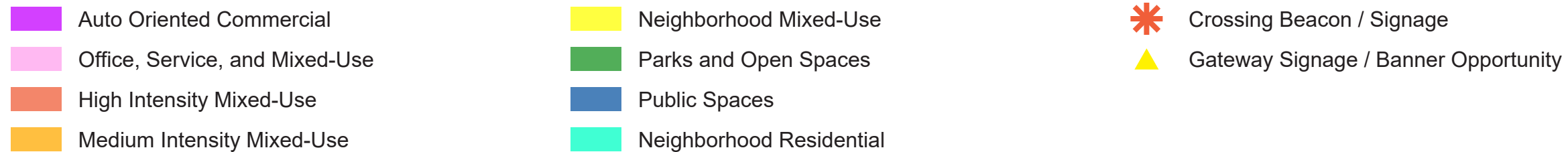
The following pages and rendering summarize the framework and strategies identified for the West Gateway. The rendering below represents a graphical illustration of the planning level design identified through this study. Specific design features will be refined through the design process, and modifications from the graphic below are anticipated.



Wayzata West Gateway



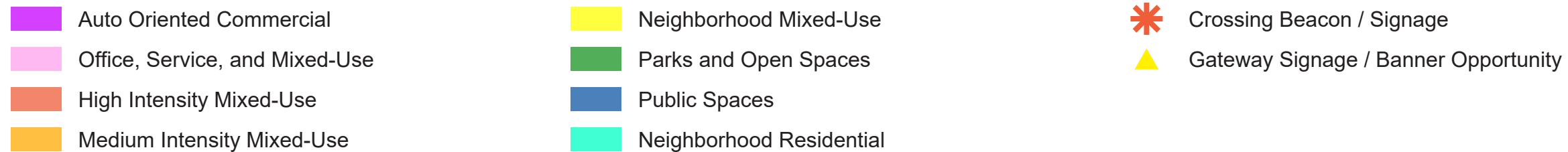
- 1 **Close Sidewalk Gaps**
Close sidewalk gaps between Ferndale Rd and the Highway 12 overpass.
- 2 **Gateway Opportunity**
Identify opportunities for gateway signage and banners at the West Gateway.
- 3 **Omit Channelized Rights**
Omit channelized right turns at Ferndale Rd and replace with mountable truck aprons to lower vehicle speeds and to make pedestrian crossings safer.
- 4 **Regional Trail Connection**
Connect to the Luce Line State Trail by closing the bikeway gap between Ferndale Rd and Barry Ave.
- 5 **Extend Median**
Extend planted median west of Minnetonka Ave along Wayzata Blvd.



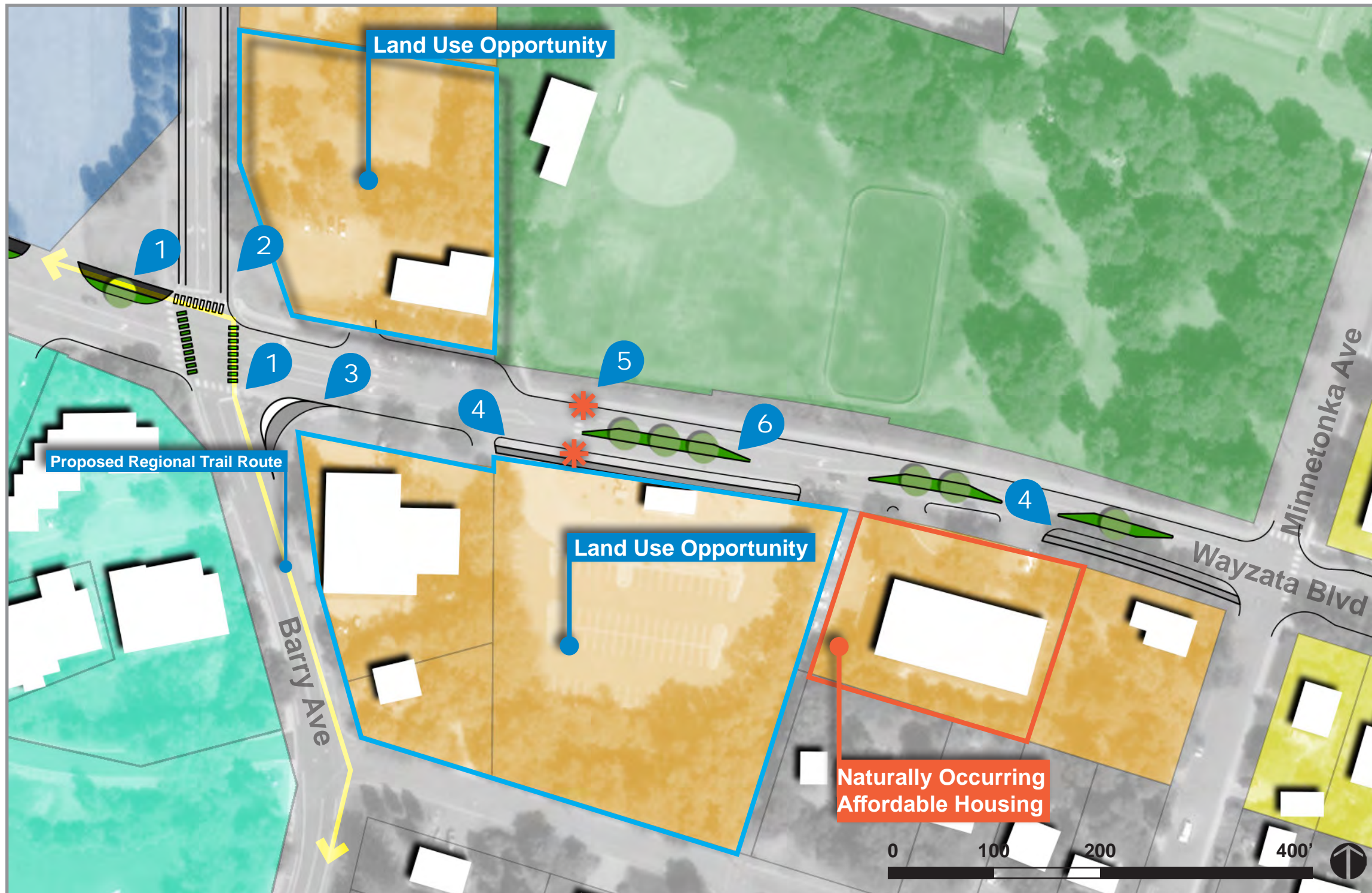
Wayzata West Gateway



- 1 **Regional Trail Connection**
Connect to the Luce Line State Trail by closing the bikeway gap between Ferndale Rd and Barry Ave.
- 2 **Extend Median**
Extend planted median west of Minnetonka Ave along Wayzata Blvd.













Wayzata West Gateway



- 1 **Regional Trail Connection**
Connect to the Luce Line State Trail by closing the bikeway gap between Ferndale Rd and Barry Ave.
- 2 **Continue Bike Lanes**
Continue the bike lanes north of Wayzata Blvd to improve safety and connections for cyclists.

Add Bike Crosswalks
Add bike crosswalks to communicate that bike facilities continue across Wayzata Blvd.
- 3 **Omit Channelized Right**
Omit channelized right turns at Barry Ave and replace with mountable truck aprons where applicable to lower vehicle speeds and to make pedestrian crossings safer.
- 4 **Close Sidewalk Gaps**
Close sidewalk gaps on the south side of Wayzata Blvd between Barry Ave and Minnetonka Ave.
- 5 **Upgrade to RRFB (Rectangular Rapid Flashing Beacon) Sign**
Upgrade the crosswalk sign to a rectangular rapid flash beacon to improve pedestrian visibility.
- 6 **Extend Median**
Extend planted median west of Minnetonka Ave along Wayzata Blvd.

| | | |
|---|--|--|
|  Auto Oriented Commercial |  Neighborhood Mixed-Use |  Crossing Beacon / Signage |
|  Office, Service, and Mixed-Use |  Parks and Open Spaces |  Gateway Signage / Banner Opportunity |
|  High Intensity Mixed-Use |  Public Spaces | |
|  Medium Intensity Mixed-Use |  Neighborhood Residential | |



PART D: IMPLEMENTATION PLAN

Part D: Implementation Plan

The actions identified within the section were built from community input, staff recommendations, and technical analysis (e.g., land use framework, roadway design recommendations, etc.).

HOW TO USE THE IMPLEMENTATION PLAN

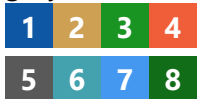


Implementation actions are presented in the following pages by plan focus area (e.g., transportation and mobility) and include regulatory, programming, planning and guidance actions, and strategies. To support implementation of each action, supporting information is provided. This information represents the most recent data available. Additionally, the implementation plan is bucketed into three timelines to gauge the overall pace of action. This implementation plan may not represent an exhaustive list of actions and should be reviewed and updated as needed to reflect the needs of the community.

IMPLEMENTATION ACTION TIMELINE:

| Timeline Category | General Timeline | Actions for Consideration |
|----------------------|------------------|--|
| Short Term Actions | 0-4 years | <ul style="list-style-type: none">• Primary safety improvements within Wayzata controlled right-of-way (ROW)• Land Use policy implementation or regulatory updates• Public Realm policy updates and framework |
| Medium Range Actions | 4-10 years | <ul style="list-style-type: none">• Safety improvements within Hennepin County ROW• Transportation improvements within Wayzata ROW• Development action support and implementation• Public Realm investments |
| Long Range Actions | 10+ years | <ul style="list-style-type: none">• Transportation improvements within Hennepin County ROW• Development action support and implementation• Public Realm investments |

PART D: IMPLEMENTATION PLAN

IMPLEMENTATION PLAN ELEMENTS TO CONSIDER:

| Category | Description | How is this displayed? |
|--------------------------------|---|--|
| Action Type | <p>Identifies the general action type from the following categories:</p> <ul style="list-style-type: none"> • Regulatory: development of policy or ordinance • Program: creation or maintenance of program • Planning: action that require additional planning or exploration for implementation • Guidance: general guidance to be considered during the decision-making process • Infrastructure: construction of infrastructure improvements | <p>Notation of the type of action: Regulatory, Program, Planning, Guidance, or Infrastructure</p> |
| Supporting Principle(s) | <p>Which of the eight established guiding principles does this action support?</p> | <p>Each supported principle is highlighted in color and unsupported principles are grey.</p>  |
| Complexity | <p>How complex is the implementation of the action? This category identifies the overall complexity for achieving success for each action. Factors that inform a low, medium, or high rating of complexity include cost, coordination with partners, and the completion of additional studies.</p> | <p>The level of complexity is provided on a scale of low, medium, and high.</p>  |
| Benefit | <p>What is the overall benefit of the implementation of the action? This category identifies the overall level of benefit of the action for the community. Factors that inform a low, medium, or high benefit rating include those that directly support progress of the Wayzata Boulevard Corridor Vision and Guiding Principles and increase the city's capacity for success.</p> | <p>The range of benefit complexity is provided on a scale of low, medium, and high, identified by the colored circles.</p>  |
| Partners | <p>What partners can assist in carrying out the action? This category identifies partners that may support the implementation of this action. This is not meant to be an exhaustive list.</p> | <p>Potential partners have been identified by name.</p> |

PART D: IMPLEMENTATION PLAN

IMPLEMENTATION ACTIONS

SHORT TERM IMPLEMENTATION ACTIONS (0 TO 4 YEARS)

| Action | Action Type | Supporting Principle(s) | Complexity | Benefit | Partners | | | | | | | | |
|--|-------------|---|------------|---------|----------|---|---|---|---|---|-------|-------|------------------------------|
| Update the height requirements within appropriate mixed-use districts to allow up to 4 stories in the areas identified for High Intensity Mixed-Use. | Regulatory | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | ● ○ ○ | ● ● ○ | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Evaluate the effectiveness of the home occupation uses within the R-3 district and make updates as warranted. | Regulatory | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | ● ○ ○ | ● ● ○ | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Update zoning ordinance to establish the Boulevard and Bluff District front yard setbacks introduced in this plan. | Planning | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | ● ○ ○ | ● ○ ○ | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Maintain the consideration for a Drive Thru through Conditional Use Permit only. | Regulatory | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | ● ○ ○ | ● ● ○ | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Continue coordination with Metro Transit regarding the potential redevelopment of the Park-and-Ride site. | Planning | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | ● ● ○ | ● ● ○ | Metro Transit |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Support the due diligence work of Metro Transit, including market research efforts. | Planning | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | ● ● ○ | ● ● ● | Metro Transit Wayzata HRA |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |

PART D: IMPLEMENTATION PLAN

| Action | Action Type | Supporting Principle(s) | Complexity | Benefit | Partners | | | | | | | | |
|---|----------------|---|------------|---------|----------|---|---|---|---|---|--|--|-----------------|
| Regularly review and update parking requirements, including the provision of shared parking. | Regulatory | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Explore an access management policy and guidance to support the consolidation of access points along Wayzata Boulevard through the development process. | Regulatory | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Continue to support connections to natural areas either through the development process or city investment. | Planning | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Complete the short-term improvements identified at the Wayzata Boulevard and Superior Avenue intersection. | Infrastructure | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Complete the short-term improvements identified at the Wayzata Boulevard and Broadway Avenue intersection. | Infrastructure | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Actively coordinate with Hennepin County regarding future improvements to Wayzata Boulevard and Central Avenue, including the identification of improvements within the County's Improvement Program. | Planning | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Hennepin County |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |

PART D: IMPLEMENTATION PLAN

| Action | Action Type | Supporting Principle(s) | Complexity | Benefit | Partners | | | | | | | | |
|--|----------------|---|------------|---------|----------|---|---|---|---|---|--|--|----------------------------|
| Coordinate with Hennepin County for the restriping of Central Avenue as identified with future resurfacing efforts. | Infrastructure | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Hennepin County |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Explore funding opportunities and program the West Gateway improvements within the city's Capital Improvement Program. | Program | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Coordinate with Three Rivers Park District on the identification of future regional trails. | Planning | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Three Rivers Park District |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Update the Wayzata Wayfinding Plan to include specific direction for Wayzata Boulevard and include gateway guidance and standards. | Planning | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Expand the city's brand standards to include any permanent and temporary signage as identified within the gateway plan. | Program | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Explore public art opportunities, identifying the first location for an installation. | Infrastructure | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Create and refine design standards for public realm improvements including sidewalk/trail design, ADA improvements, furnishing areas, etc. | Program | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |

PART D: IMPLEMENTATION PLAN

| Action | Action Type | Supporting Principle(s) | Complexity | Benefit | Partners | | | | | | | | |
|---|----------------|---|------------|---------|----------|---|---|---|---|---|--|--|-----------------|
| Review existing building design standards as a tool for affordable housing/sustainable development, and ensuring that regulations align with the gateway plan and sense of place. | Program | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Incorporate Wayzata Boulevard streetscape improvements (e.g., banner signage, gateway signage) in the Capital Improvement Program for future installation within city right-of-way. | Program | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Identify a public art pilot project within existing right-of-way. | Infrastructure | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Explore the creation of a public art commission and create programming for permanent and temporary installations. | Planning | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |

PART D: IMPLEMENTATION PLAN

MEDIUM RANGE IMPLEMENTATION ACTIONS (5 TO 10 YEARS)

| Action | Action Type | Supporting Principle(s) | Complexity | Benefit | Partners | | | | | | | | |
|---|----------------|---|------------|---------|----------|---|---|---|---|---|-----|-----|--|
| Continue to coordinate with Metro Transit regarding redevelopment of the Park-and-Ride Site. | Planning | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | ●●○ | ●●○ | Metro Transit |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Support tools needed to reinforce the redevelopment of the Park-and-Ride site. | Planning | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | ●●○ | ○●○ | Metro Transit Metropolitan Council HRA |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Explore grant opportunities and partnership opportunities to externally support regulatory updates or development activities. | Planning | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | ●●● | ○●○ | Metropolitan Council HRA |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Establish policies to guide a mix of housing typologies within the Medium and High Intensity Mixed-Use categories (e.g., market rate, high end, etc.) | Program | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | ●●○ | ○●○ | HRA |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Complete the interim improvements identified at the Wayzata Boulevard and Central Avenue intersection. | Infrastructure | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | ●●● | ●●● | Public Approval Hennepin County |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Complete the interim improvements identified at the Wayzata Boulevard and Bushaway Road intersection. | Infrastructure | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | ●●● | ●●● | Public Approval Hennepin County |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |

PART D: IMPLEMENTATION PLAN

| Action | Action Type | Supporting Principle(s) | Complexity | Benefit | Partners | | | | | | | | |
|--|----------------|---|------------|---------|----------|---|---|---|---|---|--|--|------------------------------------|
| Construct roadway improvements to Wayzata Boulevard within City of Wayzata owned right of way. | Infrastructure | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Construct sidewalk connections to close sidewalk gaps within the West Gateway and Town Core. | Infrastructure | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Install wayfinding signage and infrastructure as identified within the wayfinding and gateway plan. | Infrastructure | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Install gateway signage and infrastructure as identified within the wayfinding and gateway plan. | Infrastructure | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Explore opportunities for community events to be located along Wayzata Boulevard. | Planning | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Develop a bi-annual property owner conversation/focus group for commercial and residential properties to provide an avenue of communication. | Program | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval Property Owners |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Complete the short-term improvements identified at the Wayzata Boulevard and Barry Avenue intersection. | Infrastructure | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |
| Complete the short-term improvements identified at the Wayzata Boulevard and Ferndale Road intersection. | Infrastructure | <table border="1"> <tr> <td>1</td> <td>2</td> <td>3</td> <td>4</td> </tr> <tr> <td>5</td> <td>6</td> <td>7</td> <td>8</td> </tr> </table> | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | | | Public Approval |
| 1 | 2 | 3 | 4 | | | | | | | | | | |
| 5 | 6 | 7 | 8 | | | | | | | | | | |

PART D: IMPLEMENTATION PLAN

LONG RANGE IMPLEMENTATION ACTIONS (10 OR MORE YEARS)

| Action | Action Type | Supporting Principle(s) | Complexity | Benefit | Partners |
|---|----------------|-------------------------|------------|---------|------------------------------------|
| Update the Wayzata Boulevard Corridor Study to align with the 2050 Comprehensive Plan update. | Planning | 1 2 3 4 | | | Community |
| | | 5 6 7 8 | | | |
| Construct roadway improvements to Wayzata Boulevard within County owned right-of-way. | Infrastructure | 1 2 3 4 | | | Public Approval Hennepin County |
| | | 5 6 7 8 | | | |
| Construct roadway improvements to Central Avenue within County owned right-of-way. | Infrastructure | 1 2 3 4 | | | Public Approval Hennepin County |
| | | 5 6 7 8 | | | |
| Coordinate with MnDOT regarding regional transportation, including a potential Highway 12 interchange at Ferndale Road. | Planning | 1 2 3 4 | | | MnDOT City of Plymouth |
| | | 5 6 7 8 | | | |
| Update gateway and wayfinding signage as needed and establish a maintenance program. | Infrastructure | 1 2 3 4 | | | Public Approval |
| | | 5 6 7 8 | | | |
| Coordinate with MnDOT regarding regional transportation, including a potential Highway 12 interchange at Ferndale Road. | Planning | 1 2 3 4 | | | MnDOT City of Plymouth |
| | | 5 6 7 8 | | | |

Appendix A: Community Engagement Feedback

Phase 1 Engagement Summary

Phase 2 Engagement Summary

WAYZATA BOULEVARD CORRIDOR STUDY

ENGAGEMENT SUMMARY - AUGUST - OCTOBER 2022

PROJECT DESCRIPTION

The City of Wayzata is leading the Wayzata Boulevard Corridor Study and looking to transform the area into an even more welcoming, safe and connected corridor for the Wayzata community. The corridor study will establish a vision that extends beyond the curb line and provides a cohesive framework for land use, development regulation, future roadway and sidewalk infrastructure and streetscape improvements, as well as near-term and long-term safety improvements for pedestrians and motorists.



WHAT ENGAGEMENT OCCURRED?



Pop up Event

September 11, James J. Hill Days



Business and Organization Stakeholders Meeting


October 17, Wayzata Community Room



Community Survey


August 29 - October 14

217 Responses



WHAT WE HEARD

The Wayzata Boulevard Corridor Study conducted an initial round of community engagement to identify potential corridor opportunities and confirm existing issues along the corridor. The study team engaged with residents, business owners, and visitors to the area in multiple ways from September through October 2022, including a pop-up event, online community survey, door knocking to businesses and organizations along the corridor, yard signs and broad promotion, as well as a stakeholder meeting at the Wayzata Community Room.



WHAT SHOULD WE PRIORITIZE ABOUT WAYZATA BOULEVARD?



- **Better pedestrian connections**
- **Traffic safety**
- **Roadway design and appearance**

The two biggest concerns:

- **Access management** (pulling in and out of driveways and business entrances)
- **Safety** for all modes, including motorist, pedestrians and bicyclists.



PEOPLE WANT TO SEE MORE



Parks



Dining and Entertainment



Everyday Shopping



Personal Services



Arts and Cultural Opportunities

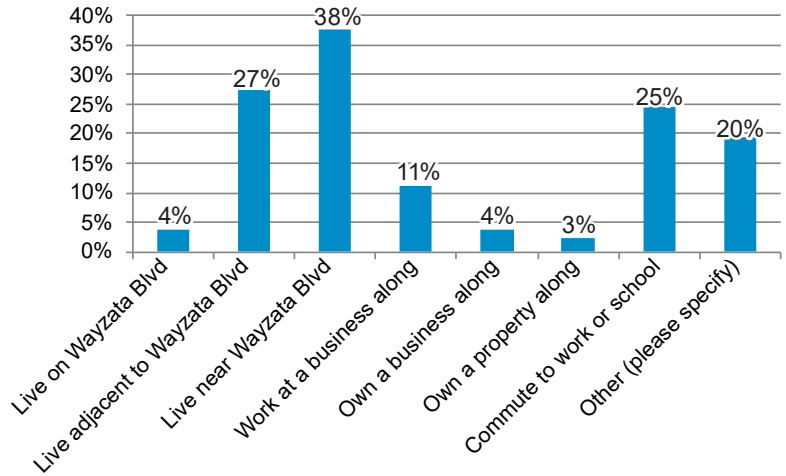




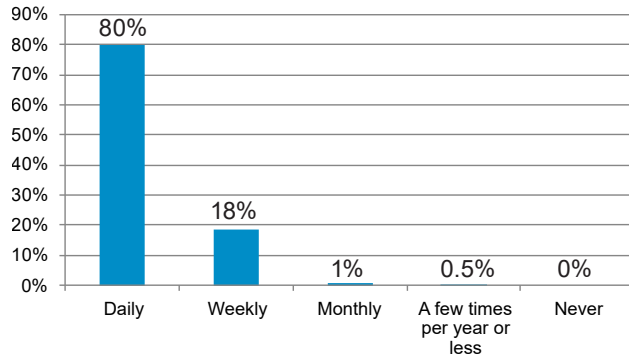
SURVEY RESULTS

An online survey was available from August 29 - October 14. The survey asked the public what was important to them about Wayzata Boulevard, what issues should be prioritized, and what they would like to see more of on Wayzata Boulevard. See survey results below.

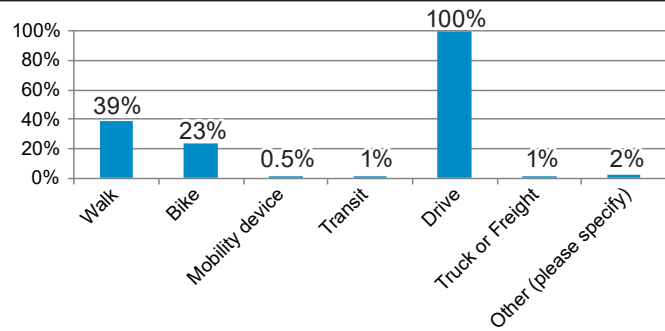
1) Where do respondents live?



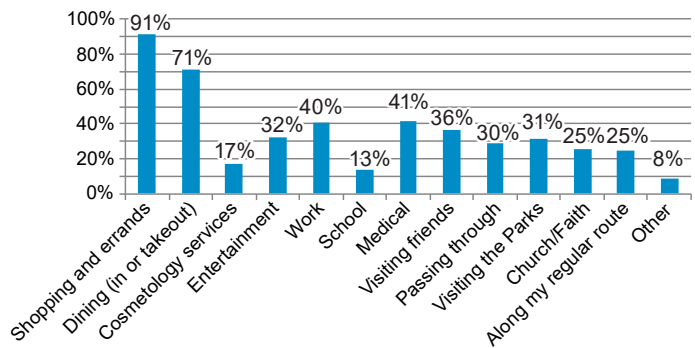
2) How often do respondents travel on Wayzata Blvd?



3) How do respondents travel on Wayzata Blvd or Central Ave N?



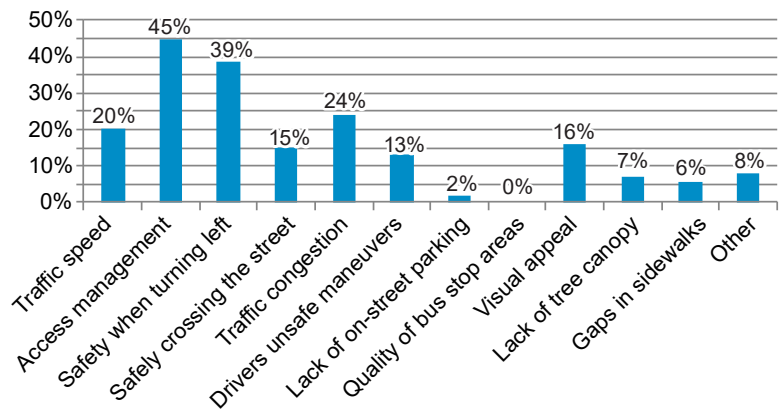
4) What are the primary purposes for travel on Wayzata Blvd or Central Ave N?





SURVEY RESULTS

5) What are the top concerns about the Wayzata Blvd?



6-7) How should we prioritize transportation modes for each section?

Wayzata Blvd to Highway 12

- 1) Car/Vehicle
- 2) Walk
- 3) Mobility Device
- 4) Transit
- 5) Bike

Superior Blvd to Highway 12

- 1) Car/Vehicle
- 2) Walk
- 3) Mobility Device
- 4) Transit
- 5) Bike

Minnetonka Ave to Superior Blvd

- 1) Car/Vehicle
- 2) Walk
- 3) Mobility Device
- 4) Transit
- 5) Bike

Highway 12 to Minnetonka Ave

- 1) Car/Vehicle
- 2) Walk
- 3) Mobility Device
- 4) Transit
- 5) Bike



SURVEY RESULTS



8-9) What comes to mind for each section of Wayzata Blvd? What should be changed or improved for each section of Wayzata Blvd?

Wayzata Blvd to Highway 12

Frequently mentioned

- Congestion
- Traffic
- Bad turns
- Dangerous
- Accidents

Changes or improvements

- Turn lanes
- Landscaping
- Traffic management
- Pedestrian friendly options

Superior Blvd to Highway 12

Frequently mentioned

- Congestion
- Dangerous
- Busy
- Businesses
- Unsafe

Changes or improvements

- Safer turns
- More pedestrian facilities
- Ease of business access

Minnetonka Ave to Superior Blvd

Frequently mentioned

- Too many cars
- Residential
- Nice trees

Changes or improvements

- Preserve current feel
- Safer for walkers

Highway 12 to Minnetonka Ave

Frequently mentioned

- Good flow
- Less traffic

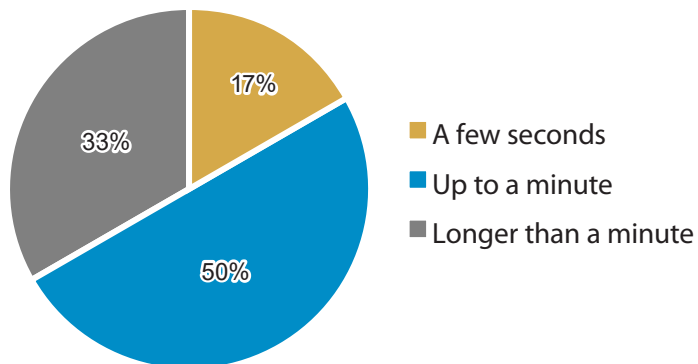
Changes or improvements

- More landscaping
- Emphasize walkers and bikers
- Lower speed limit



SURVEY RESULTS

12) How long do you typically have to wait to cross Wayzata Blvd or Central Ave?



13) Why do you visit Wayzata Blvd?

Most important reasons people visit the corridor.

1. Everyday shopping
2. Dining and entertainment
3. Parks
4. Banking
5. Health care services
6. Consumer goods
7. Specialty shops
8. Personal services
9. Government services
10. Employment
11. Church, faith communities
12. Arts & cultural opportunities
13. Sports facilities & events
14. Accounting and legal services

14) What do you want to see more of on and around Wayzata Blvd?

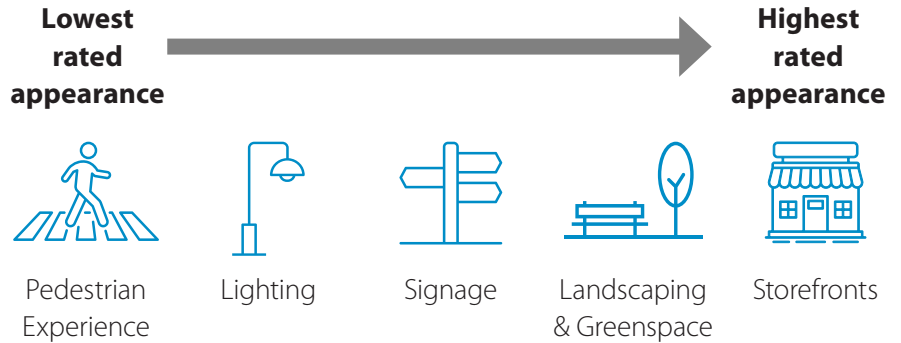
Top priorities identified.

1. Dining and entertainment (tie)
1. Parks (tie)
3. Everyday shopping
4. Specialty shops
5. Arts & cultural opportunities
6. Health care services
7. Personal services
8. Consumer goods
9. Employment
10. Banking
11. Government services
12. Church, faith communities
13. Sports facilities & events
14. Accounting and legal services



SURVEY RESULTS

15) Please rate (1 low - 5 high) the current appearance of different items along Wayzata Blvd



16) What issues or opportunities should be prioritized along Wayzata Blvd and Central Ave N?

Top priorities identified

1. Better pedestrian experience
2. Roadway design and appearance
3. Traffic safety
4. Better bicycle options
5. Accessibility improvements for seniors and disabled people
6. More dining options
7. More opportunities to socialize and interact
8. Public Art/Gateways
9. Improved parking
10. Changes to the mix of businesses
11. Different housing choices


WAYZATA BOULEVARD CORRIDOR STUDY


ENGAGEMENT SUMMARY - FEBRUARY TO MARCH 2023

PROJECT DESCRIPTION

The City of Wayzata is leading the Wayzata Boulevard Corridor Study and looking to transform the area into an even more welcoming, safe and connected corridor for the Wayzata community. This phase of engagement provided multiple opportunities to share project updates and gather input to inform strategies and recommended actions. Input received at pop-ups and the open house was closely aligned and summarized below.

| | | | |
|--|---|---|--|
|  <p>WHAT ENGAGEMENT OCCURRED?</p> |  <p>Pop up Events</p> <p>February 16 & March 2, Lunds-Byerlys</p> |  <p>Open House Event</p> <p>March 14, Wayzata Community Room</p> | <p>Engaged 100+ Community Members</p> |
|--|---|---|--|

| | |
|--|---|
|  <p>ENGAGEMENT OVERVIEW</p> | <p>This phase of engagement opened the conversation with community members to dig into the specific improvements and recommendations for the Wayzata Boulevard Corridor Study. Two pop-up events were held at Lunds-Byerlys to gather to interact with community members, inform them of the project, and gather input. A community Open House was held from 4 to 6pm at the Wayzata Community Room on March 14, 2023. The community open house included a short presentation at 4:15 and 5:15 and attendees were able to engage with a number of activities in the room. Online attendees were also welcomed via Zoom.</p> |
|--|---|

| | |
|---|---|
|  <p>WHAT WERE THE COMMON THEMES?</p> | <ul style="list-style-type: none"> • The development and built environment along Wayzata Boulevard create an opportunity to maintain the charm and services needed for the community. It should have its own identity. • Infrastructure to support movement by all modes (e.g., walking, biking, and driving) is important, but the safety for everyone must be prioritized. • Enhancing the plantings, landscaping and green spaces along the corridor, along with access to parks and recreation, is desired. |
|---|---|

| | | | | | | |
|---|---|--|--|--|--|---|
|  <p>WHERE ARE INVESTMENTS WANTED?</p> | <table border="0"> <tr> <td data-bbox="479 1543 609 1711">  <p>Additional Sidewalks and Trails</p> </td> <td data-bbox="673 1543 836 1711">  <p>Improved Crossings for Bikes and Pedestrians</p> </td> <td data-bbox="885 1543 1047 1711">  <p>Increased plantings and landscaping</p> </td> <td data-bbox="1096 1543 1258 1711">  <p>Investments in Parks and Open Spaces</p> </td> <td data-bbox="1307 1543 1485 1711">  <p>Improvements to sight-lines</p> </td> </tr> </table> |  <p>Additional Sidewalks and Trails</p> |  <p>Improved Crossings for Bikes and Pedestrians</p> |  <p>Increased plantings and landscaping</p> |  <p>Investments in Parks and Open Spaces</p> |  <p>Improvements to sight-lines</p> |
|  <p>Additional Sidewalks and Trails</p> |  <p>Improved Crossings for Bikes and Pedestrians</p> |  <p>Increased plantings and landscaping</p> |  <p>Investments in Parks and Open Spaces</p> |  <p>Improvements to sight-lines</p> | | |



ENGAGEMENT RESULTS

Open House attendees were asked to create their own vision statement for Wayzata Boulevard by filling in the blanks or creating their own statement. The following statements were created.

Statement 1

Wayzata Boulevard is a corridor that is **traffic heavy** and **polluted** and is designed for **cars and buses**.

Wayzata Boulevard is a corridor that is **safe** and **walkable** and is designed for **pedestrians and locals**.

Wayzata Boulevard is a corridor that is **greener** and **quieter** and is designed for **shopping, eating, and slower travel**.

Wayzata Boulevard is a corridor that is **safe** and is designed for **children in the neighborhood to walk to school and families to bike down to the trails**.

Wayzata Boulevard is a corridor that is **safe** and **easy** and is designed for **walking and driving**.

Wayzata Boulevard is a corridor that is **safe** and **pedestrian friendly** and is designed for **directing traffic through or to the lake and restaurants**.

Statement 2

Wayzata Boulevard is **defining** to the community, serving as **service businesses** and **residential housing**.

Wayzata Boulevard is **main corridor** to the community, serving as **family** and **neighbors**.

Wayzata Boulevard is **necessary** to the community, serving as **a through-way** and **a beautiful site**.

Statement 3

The identity of Wayzata Boulevard is showcased through **safety** and **availability**.

The identity of Wayzata Boulevard is showcased through **upgrades under development, commercial east of Central Avenue**.

The identity of Wayzata Boulevard is showcased through **median trees** and **controlled speed**.

The identity of Wayzata Boulevard is showcased through **too little green space, unpleasant sidewalk right up against to busy road**.



ENGAGEMENT RESULTS

Open House attendees were asked what investments they would like to see made to the public realm or streetscape along the corridor. The following ideas were generated.

Connecting people to destinations within the community.



No bike lanes on Wayzata Blvd between 101 and Bushaway. Way too dangerous! This is a main route in and out of Wayzata.

Fully connected sidewalk system

Functionality of road (still need to get to businesses)

Not need a car to get around, that way we can journey instead of being in a metal box (car)

Safe for pedestrians as a priority over bikes and traffic speeds

Creating places and spaces for people to connect.



Festivals and community events that close the street for bikes and peds only.

Fewer cars and more bikes will lead to greater independence for children using parks and school.

Connect to the Big Woods

Enhancing the sense of place and identity for Wayzata and the Wayzata Boulevard corridor.



Have reasonable rent rates for vendors. There is virtually no where to sit and have coffee west of 101.

Creating an safe and welcoming environment.



Intersection of Benton and Superior

Crossing of Central - scooters

Don't reduce traffic lanes. If you do, it will be congested and dangerous as Lake Street is now

Outdoor Cafes

Ped Safety. Ped crossings west of Central

Reduce traffic lanes to one each way for safety and making left turns easier. All 6 roads connect are 1 lane

Slow traffic down

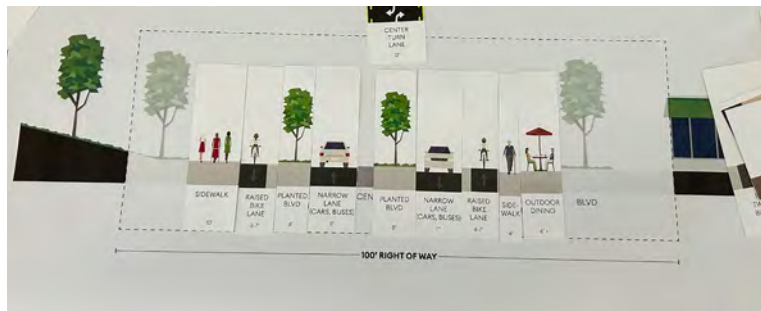
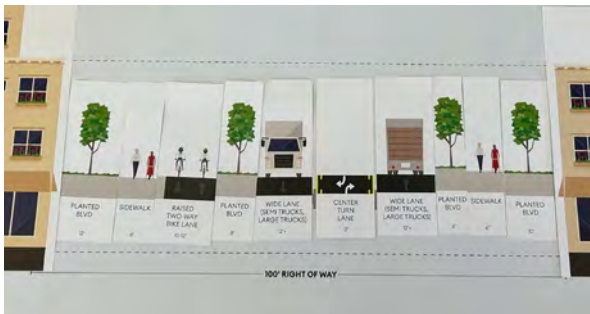
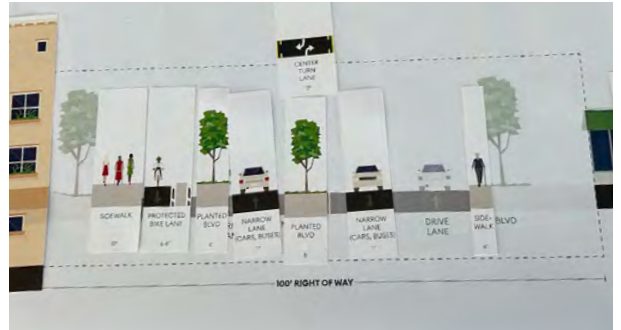
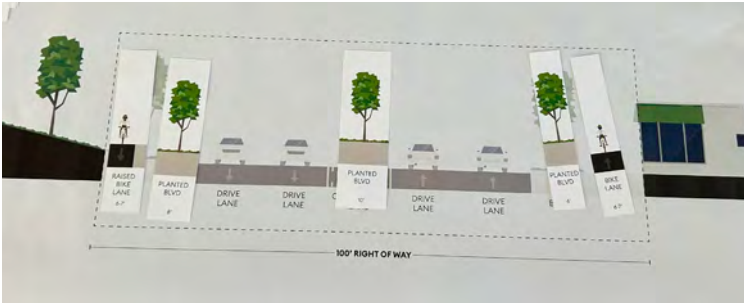
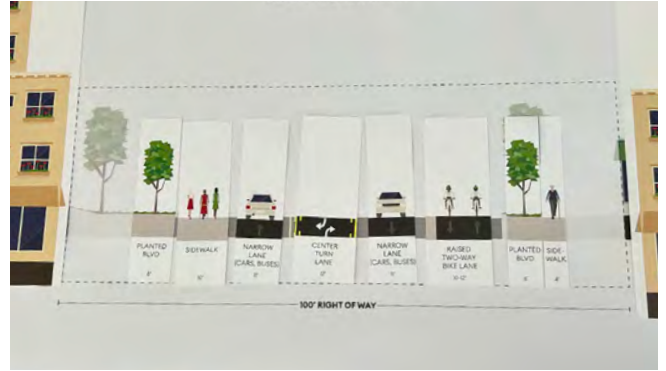
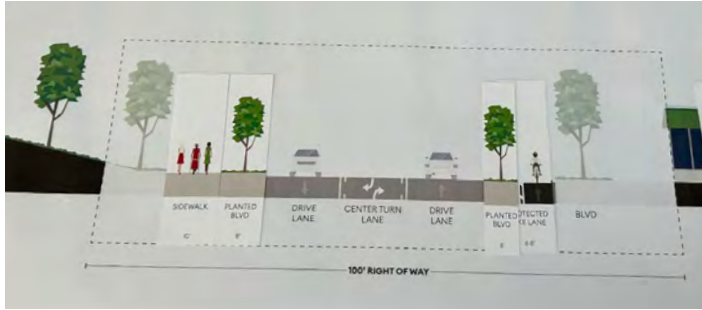
Traffic Calming

The easier it is to drive "level of service" the more people will drive it. No speed limit increase has ever permanently decreased traffic. Lower speeds, safer streets.



ENGAGEMENT RESULTS

Open House attendees were asked to design their own Wayzata Boulevard between Central and Bushaway Rd. They were providing a figure of the existing design and were provided various design elements to create an updated roadway design.



Appendix B: CSC Charrette Summaries

Vision Meeting: August 2022

Land Use Charrette: November 2022

Transportation Charette: April 2022

Key Theme Exercise

Wayzata Boulevard Corridor Study



LAND USE

Redevelopment, new development, building materials, sense of place.

| | Topic: | Description: | Effort: | Priority: |
|----------|---|---|--|--|
| A | | Use land use to reinforce village character at east entry (Central/Bushaway) | <input type="checkbox"/> Low Hanging Fruit <input checked="" type="checkbox"/> Aspirational | <input checked="" type="checkbox"/> 1 2 3 4 5 |
| B | | Use land use to build holistic community with residents driving retail, not just commerce | <input type="checkbox"/> Low Hanging Fruit <input checked="" type="checkbox"/> Aspirational | <input checked="" type="checkbox"/> 1 2 3 4 5 |
| C | Redevelopment | Density, affordable housing/mixed use | <input type="checkbox"/> Low Hanging Fruit <input checked="" type="checkbox"/> Aspirational | <input type="checkbox"/> 1 2 <input checked="" type="checkbox"/> 3 4 5 |
| D | Transit center, expand the park (meaningful to local residents) | | <input type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | <input type="checkbox"/> 1 2 3 4 5 |
| E | Building form | Active ground floor | <input type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | <input type="checkbox"/> 1 2 3 4 5 |
| F | Housing above | | <input type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | <input type="checkbox"/> 1 2 3 4 5 |
| G | Affordable, small size units | | <input type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | <input type="checkbox"/> 1 2 3 4 5 |
| H | No more drive thrus | | <input type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | <input type="checkbox"/> 1 2 3 4 5 |
| I | Colonial Square | Mixed use development – neighborhood services | <input type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | <input type="checkbox"/> 1 2 3 4 5 |
| J | 4-5 stories on east segment | Redevelop with housing | <input type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | <input type="checkbox"/> 1 2 3 4 5 |



MOBILITY

Travel around by walking, rolling, bicycling, driving, and freight, as well as parking.

| | Topic: | Description: | Effort: | Priority: |
|----------|--|---|--|--|
| A | Multimodal | Take advantage of regional trails/make Wayzata a hub for the regional network | <input type="checkbox"/> Low Hanging Fruit <input checked="" type="checkbox"/> Aspirational | <input type="checkbox"/> 1 2 3 4 5 |
| B | Mobility/Safety | Make Wayzata more walkable | <input checked="" type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | <input checked="" type="checkbox"/> 1 2 3 4 5 |
| C | Connectivity | Trails, parks, commercial, and neighborhoods | <input type="checkbox"/> Low Hanging Fruit <input checked="" type="checkbox"/> Aspirational | <input checked="" type="checkbox"/> 1 2 3 4 5 |
| D | Connectivity | Big Woods and Nature Preserve | <input type="checkbox"/> Low Hanging Fruit <input checked="" type="checkbox"/> Aspirational | <input type="checkbox"/> 1 2 3 4 <input checked="" type="checkbox"/> 5 |
| E | Bike lanes or off-street trails | Off-street on west and east segments | <input type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | <input checked="" type="checkbox"/> 1 2 3 4 5 |
| F | Sidewalk connections | Consistent, wide sidewalks | <input type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | <input type="checkbox"/> 1 2 3 4 5 |
| G | Crossing the Blvd - connecting neighborhoods in north to Lake Street | Clear and safe pedestrian crossing and nature areas/parks | <input type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | <input checked="" type="checkbox"/> 1 2 3 4 5 |
| H | Parking shared among multiple facilities | | <input checked="" type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | <input type="checkbox"/> 1 2 3 4 5 |
| I | Planning for electric vehicles, driverless vehicles | | <input type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | <input type="checkbox"/> 1 2 3 4 5 |
| J | Islands of refuge at crossings | | <input checked="" type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | <input type="checkbox"/> 1 2 3 4 5 |

Key Theme Exercise

Wayzata Boulevard Corridor Study



PUBLIC REALM & STREETScape

Pedestrian realm, lighting, landscaping, signage, wayfinding, public art, etc.

| | Topic: | Description: | Effort: | Priority: |
|----------|---|--|---|-----------|
| A | Placemaking | Incorporate unique public art (Wells Fargo corner) to create identity (in ROW by City or Condition of development, % for public art) | <input checked="" type="checkbox"/> Low Hanging Fruit <input checked="" type="checkbox"/> Aspirational | |
| B | | Define entrance through streetscape elements, gateway | <input checked="" type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | |
| C | Wayfinding | Gateway and signage, consider routing to Central | <input checked="" type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | |
| D | Nice places to sit | | <input checked="" type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | |
| E | Update benches | | <input checked="" type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | |
| F | New tree plantings and nice village street lighting | Make it feel like a village Gateway signage at city boundaries – thoughtful design and placement | <input type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | |



ROADWAY SAFETY

Opportunities for the safe movement of goods and people throughout the corridor.

| | Topic: | Description: | Effort: | Priority: |
|----------|----------------------------------|--|--|-----------|
| A | Safety | Improve pedestrian, traffic and other mode safety | <input type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | |
| B | Safety | Roundabouts (Ferndale and Barry), medians, traffic calming, pedestrian buffers, access points | <input type="checkbox"/> Low Hanging Fruit <input checked="" type="checkbox"/> Aspirational | |
| C | Access to coffee and gas station | We need these services and they are popular, though access points are confusing (scary left turns) | <input type="checkbox"/> Low Hanging Fruit <input type="checkbox"/> Aspirational | |

Issues and Opportunities Exercise

Wayzata Boulevard Corridor Study

ISSUE: An existing condition, design, or feature that creates a problem, challenge, or barrier.

OPPORTUNITY: An existing condition, design, or feature that should be expanded or added.

Highway 12 to Minnetonka Ave



Issue:

- Limited intersection conflicts
- Few crosswalks
- Few sidewalks

Details:

Only stacked stop sign in the State – issue with collisions

What should be explored to address the issue?

Opportunity:

- Ample corridor width
- Space for trails and medians
- Focus on trail connections

Details:

Trails on Barry and Ferndale

What should be explored to capitalize on the opportunity?

Issue:

- Speed and transition between rural area
- Lack of sidewalk continuity

Details:

No buffer at sidewalk; Underwhelming aesthetically; Under used park and ride

What should be explored to address the issue?

Stop sign at Barry; Curb and gutter

Opportunity:

- Park and ride development and adjacent parcel
- Regional trail connection

Details:

Tie together sidewalk connections

What should be explored to capitalize on the opportunity?

Adjacent parcels; Partnerships – Three Rivers Park District, Metro Transit; Expand park (NE corner of Barry)

Issue:

- Intersection of Ferndale – lots of idling
- Lack of sidewalk connections along Blvd
- Speed issue: 50 to 35 mph – road too wide
- Dangerous crossings at Barry, near school and park

Details:

Design speed is too high

What should be explored to address the issue?

Opportunity:

- Roundabout at Ferndale
- Build sidewalk
- Continue tree island median all the way to Highway 12
- Park and Ride site redevelopment

Details:

Housing above bus facilities at park and ride; Barry – potentially lots of crossing signage

What should be explored to capitalize on the opportunity?

Issues and Opportunities Exercise

Wayzata Boulevard Corridor Study

ISSUE: An existing condition, design, or feature that creates a problem, challenge, or barrier.

OPPORTUNITY: An existing condition, design, or feature that should be expanded or added.

Minnetonka Ave to Superior Blvd



Issue:

- Potential pedestrian improvements

Details:

Sidewalks, crossings

What should be explored to address the issue?

Opportunity:

Details:

What should be explored to capitalize on the opportunity?

Issue:

- Best practices for sidewalks and intersections

Details:

How do we get to Mill Street ramp?

What should be explored to address the issue?

- Roadway improvements
- Signage to downtown

Opportunity:

- New development

Details:

What should be explored to capitalize on the opportunity?

- Partnerships with developers

Issue:

- Not easy to bike – still high traffic speed
- Room for parking but no one needs them
- Unsafe crossings

Details:

Sidewalks, crossings

What should be explored to address the issue?

Opportunity:

- NOAH (naturally occurring affordable housing) buildings - preserve
- Save smaller units
- Add bike lanes

Details:

What should be explored to capitalize on the opportunity?

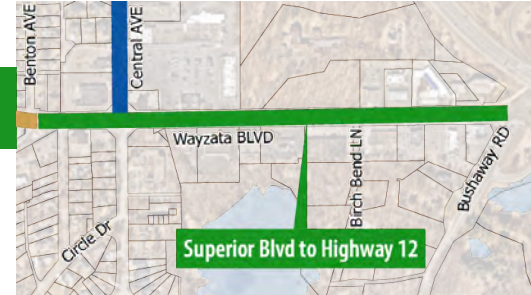
Issues and Opportunities Exercise

Wayzata Boulevard Corridor Study

ISSUE: An existing condition, design, or feature that creates a problem, challenge, or barrier.

OPPORTUNITY: An existing condition, design, or feature that should be expanded or added.

Superior Blvd to Highway 12



Issue:

- Accesses need to be cleaned up
- Central and Bushaway only crossings
- Fast traffic

Details:

What should be explored to address the issue?

Opportunity:

- Create sense of place
- Make clear you are not on Highway anymore

Details:

Expensive Option: off ramp at Ferndale to relieve traffic; Crosswalk at Birchbend to connect to new Big Woods entrance

What should be explored to capitalize on the opportunity?

- Public Art
- Crosswalks

Issue:

- Safety, barrier of road cuts town
- Vehicular oriented

Details:

Third lane confusing as driver; Sidewalk gaps; Lack of control

What should be explored to address the issue?

Calm traffic, pedestrian friendly; Density/mixed use

Opportunity:

- Gateway, land use/development

Details:

Signage to parks/Big Woods

What should be explored to capitalize on the opportunity?

Clarify access points/consolidation; Facilitate development we want

Issue:

- Not safe biking
- Not a charming street
- Lots on south side aren't deep – hard to redevelop
- Lack of housing density on corridor, lack of affordable housing

Details:

What should be explored to address the issue?

Opportunity:

- Separated off-street trail for maximum safety
- Connect Big Woods and natural area

Details:

Off-Street trail connections to neighborhoods for families; Build affordable housing 4-5 stories above commercial; Views of lake possible from upper stories on south side of lake

What should be explored to capitalize on the opportunity?

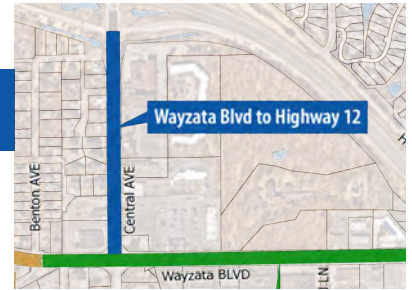
Issues and Opportunities Exercise

Wayzata Boulevard Corridor Study

ISSUE: An existing condition, design, or feature that creates a problem, challenge, or barrier.

OPPORTUNITY: An existing condition, design, or feature that should be expanded or added.

Wayzata Blvd to Highway 12



Issue:

- Improve pedestrian safety

Details:

What should be explored to address the issue?

Opportunity:

- Potential fast access to town if Wayzata is slowed

Details:

What should be explored to capitalize on the opportunity?

Issue:

- "Ugliest"
- Could get more traffic
- Safety

Details:

Pedestrian Safety

What should be explored to address the issue?

Opportunity:

- Medical building and colonial square development
- Gateway signage
- Shift highway signage to Wayzata
- Streetscape improvements

Details:

What should be explored to capitalize on the opportunity?

Issue:

- Turning into bank, pharmacy, grocery store
- Land use doesn't allow 4-5 stories

Details:

What are the traffic counts?

What should be explored to address the issue?

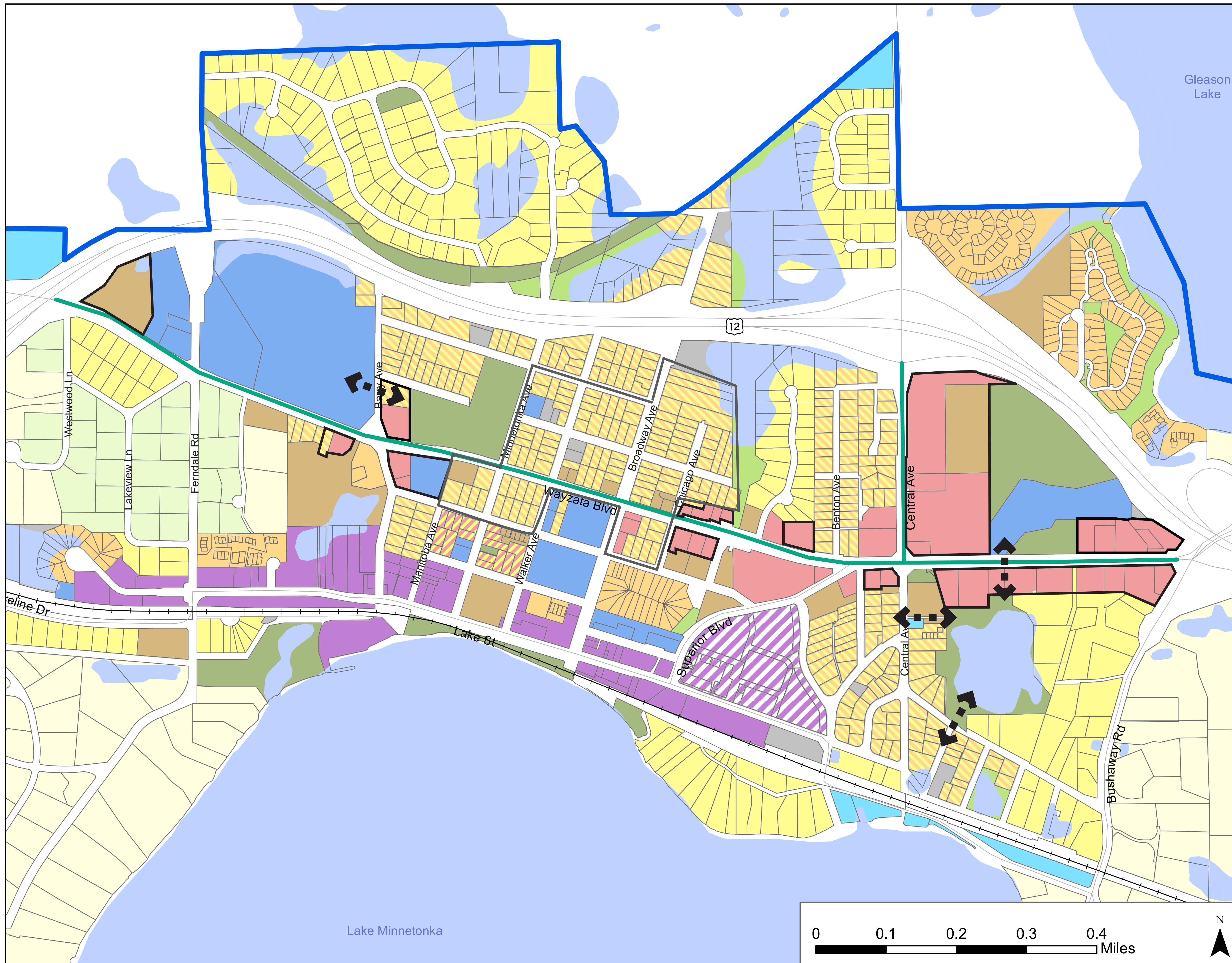
Redesign road and access points

Opportunity:

- Show that you're entering a town

Details:

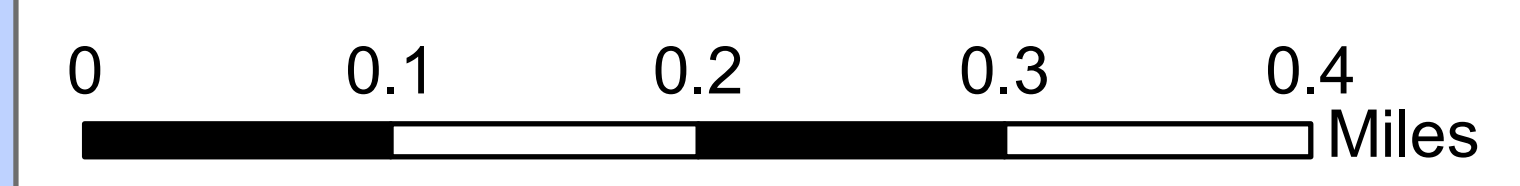
What should be explored to capitalize on the opportunity?



Existing Land Use

Legend

- Wayzata Boulevard Corridor Study Area
- Estate Single Family - 0.25 to 0.5 units/acre
- One Acre Single Family - 0.5 to 1 unit/acre
- Low Density Residential - 1 to 3 units/acre
- Central Core Residential - 3 to 6 units/acre
- Medium Density Residential - 6 to 12 units/acre
- High Density Residential - 12 or more units/acre
- Commercial
- Mixed-Use Residential and Commercial
- Central Business District - 20 or more units per acre
- Downtown Mixed-Use District - 20 or more units per acre
- Institutional/Public
- Semi-Public/Private
- Parks
- Public Open Space
- Vacant



Wayzata Boulevard Corridor Study

Highway 12 to Minnetonka Ave

Mixed-Use Commercial / Residential
 75% Residential (Condo/Higher End) and 25% Commercial
 Maximum 3 Stories with terraced setbacks.

Central Core Residential
 Residential - Single Family Residential - Row Housing/Fourplex

High Density Residential
 Residential - Small Scale Apartment Residential - Row Housing

Institutional / Open Space
 Institutional - School and Church
 Green Space/Park

Minnetonka Ave to Superior Blvd

Mixed-Use Commercial / Residential
 75% Residential (Condo/Higher End) and 25% Commercial
 Maximum 3 Stories

Residential - Row Housing

Central Core Residential
 Residential - Single Family Residential - Row Housing/Fourplex

Small Business and Office Uses (C1-A)

High Density Residential
 Residential - Small Scale Apartment Residential - Row Housing

Institutional / Open Space
 Institutional
 Green Space/Park

Superior Blvd to Highway 12

Mixed-Use Commercial / Residential
 75% Residential (Market Rate) and 25% Commercial
 Maximum 5 Stories

Central Core Residential
 Residential - Single Family Residential - Row Housing/Fourplex

High Density Residential
 Residential - Apartments

Institutional / Open Space
 Institutional
 Green Space/Park

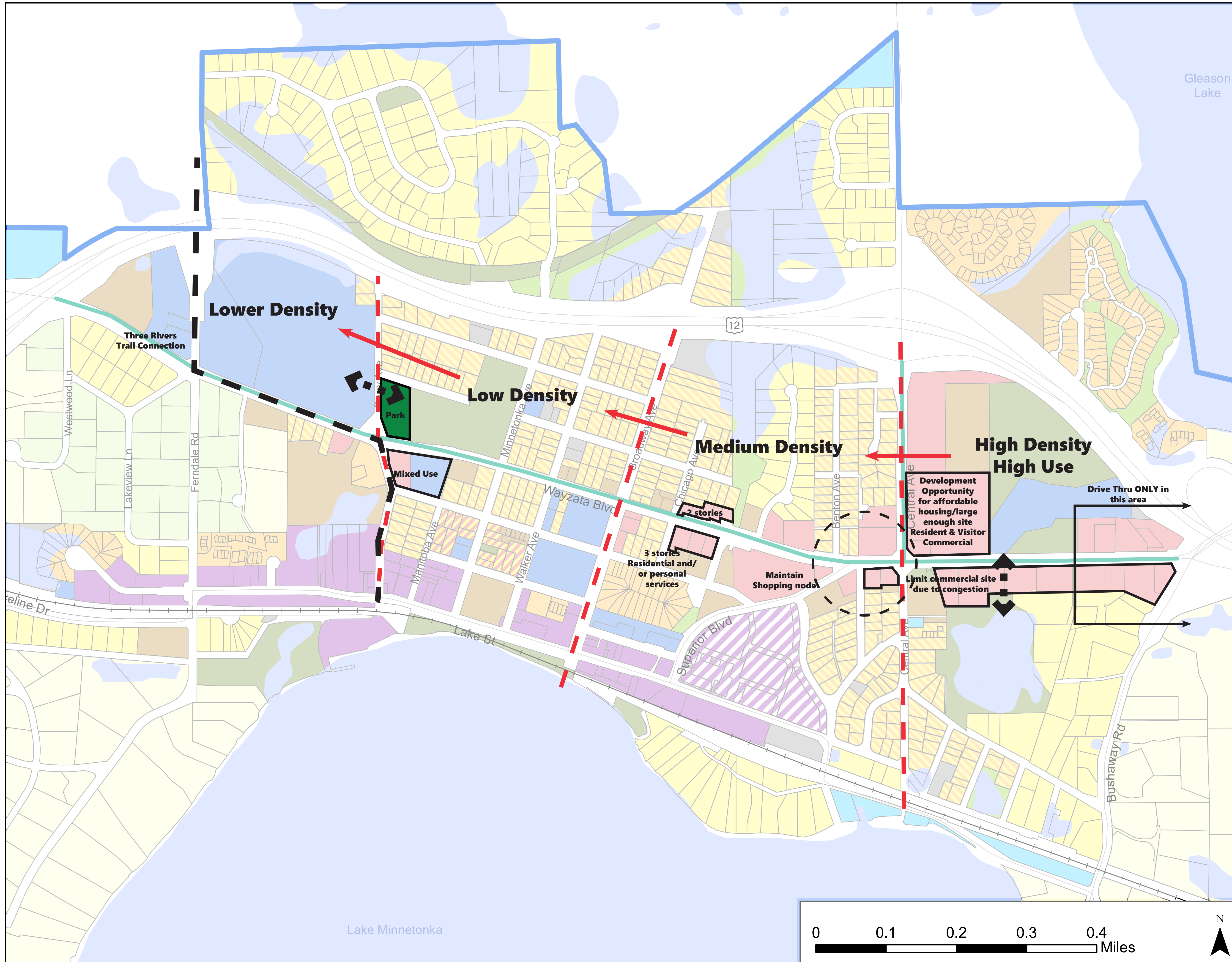
Wayzata Blvd to Highway 12

Mixed-Use Commercial / Residential
 75% Residential (Market Rate) and 25% Commercial
 Maximum 5 Stories

Central Core Residential
 Residential - Single Family Residential - Row Housing/Fourplex

High Density Residential
 Residential - Apartments

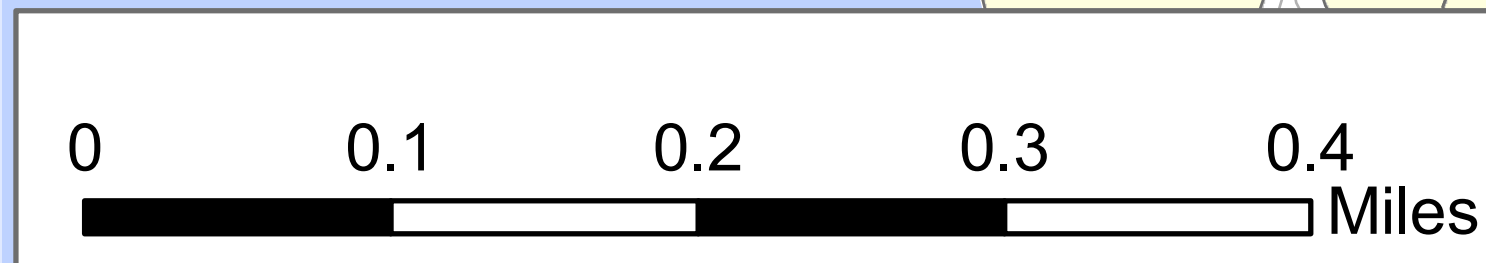
Institutional / Open Space
 Institutional
 Green Space/Park



Existing Land Use

Legend

- Wayzata Boulevard Corridor Study Area
- Estate Single Family - 0.25 to 0.5 units/acre
- One Acre Single Family - 0.5 to 1 unit/acre
- Low Density Residential - 1 to 3 units/acre
- Central Core Residential - 3 to 6 units/acre
- Medium Density Residential - 6 to 12 units/acre
- High Density Residential - 12 or more units/acre
- Commercial
- Mixed-Use Residential and Commercial
- Central Business District - 20 or more units per acre
- Downtown Mixed-Use District - 20 or more units per acre
- Institutional/Public
- Semi-Public/Private
- Parks
- Public Open Space
- Vacant



Wayzata Boulevard Corridor Study

Highway 12 to Minnetonka Ave

Mixed-Use Commercial / Residential
Tall needs setbacks
Community Opportunities

Central Core Residential
Highland Apartments

High Density Residential

Institutional / Open Space
Residential Row Housing **Buy WMS Site?!**

Minnetonka Ave to Superior Blvd

Mixed-Use Commercial / Residential
Residential Look

Central Core Residential
Allow small business/office uses
C1-A - live/work
Small Scale
Owner Occupied

High Density Residential
Keep

Institutional / Open Space

Superior Blvd to Highway 12

Mixed-Use Commercial / Residential
Green Space Park Colonial Square/
North Transit End and
Parking Hub
Ways to "Sneak Out"

Central Core Residential

High Density Residential
Best opportunity for high-density

Institutional / Open Space
Green Space Park

Wayzata Blvd to Highway 12

Mixed-Use Commercial / Residential
Medical Office **Roof Top Uses**

Central Core Residential

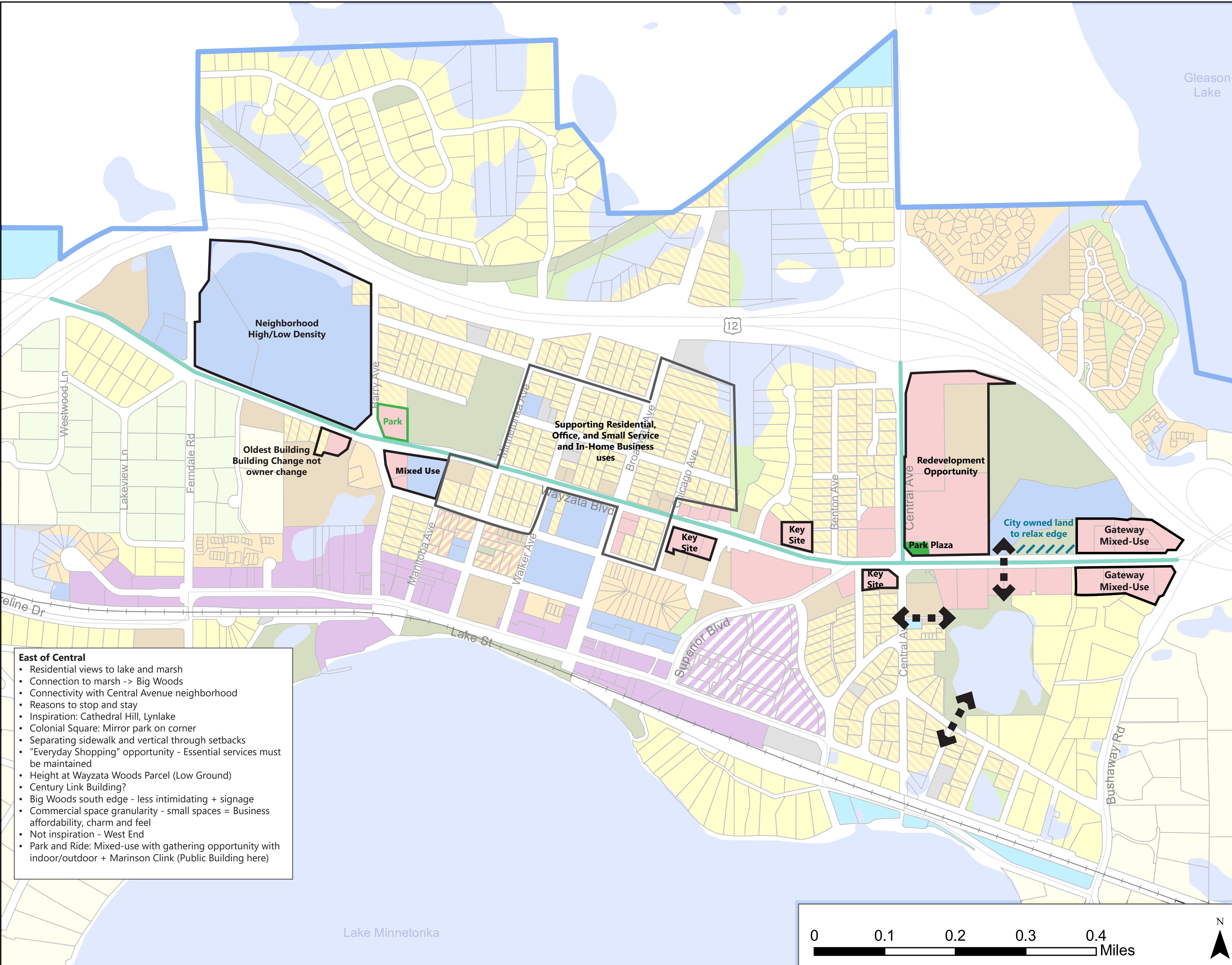
High Density Residential

Institutional / Open Space

Existing Land Use

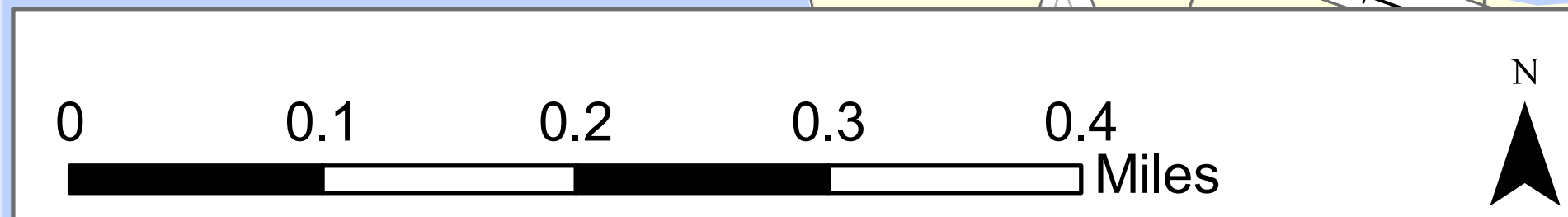
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- Semi-Public/Private
- Parks
- Public Open Space
- Vacant



East of Central

- Residential views to lake and marsh
- Connection to marsh -> Big Woods
- Connectivity with Central Avenue neighborhood
- Reasons to stop and stay
- Inspiration: Cathedral Hill, Lynlake
- Colonial Square: Mirror park on corner
- Separating sidewalk and vertical through setbacks
- "Everyday Shopping" opportunity - Essential services must be maintained
- Height at Wayzata Woods Parcel (Low Ground)
- Century Link Building?
- Big Woods south edge - less intimidating + signage
- Commercial space granularity - small spaces = Business affordability, charm and feel
- Not inspiration - West End
- Park and Ride: Mixed-use with gathering opportunity with indoor/outdoor + Marinson Clink (Public Building here)



Wayzata Boulevard Corridor Study

Highway 12 to Minnetonka Ave

Mixed-Use Commercial / Residential

75% Residential and 25% Commercial

75% Residential and 25% Commercial

Central Core Residential

Residential Single Family

Residential Row Housing

High Density Residential

Residential Row Housing

Residential Row Housing

Institutional / Open Space

School/ Church

Green Space Park

Minnetonka Ave to Superior Blvd

Mixed-Use Commercial / Residential

Residential Row Housing - Smaller units

3 Story mixed use

Medical health fitness

Central Core Residential

Residential Single Family

Residential Duplex/ Fourplex

High Density Residential

Institutional / Open Space

Superior Blvd to Highway 12

Mixed-Use Commercial / Residential

Mixed-Use Vertical Market Rate

Commercial Drive Thru

Larger comm units

Central Core Residential

High Density Residential

Best opportunity for high-density

Institutional / Open Space

Wayzata Blvd to Highway 12

Mixed-Use Commercial / Residential

Central Core Residential

High Density Residential

Institutional / Open Space



WAYZATA BOULEVARD CORRIDOR STUDY

CORRIDOR STUDY COMMITTEE MEETING #5

Thursday, April 20 | 7:30 to 9:30am | Community Room

1 INTRODUCTIONS

- Introductions

2 PROJECT UPDATES

- Project Overview, Schedule, and Recent Activities

3 PURPOSE AND OUTCOME

- What is the intent of the Transportation Charrette?
- What is the desired outcome?

4 EXISTING CONDITIONS OVERVIEW

- Existing Conditions of Wayzata Boulevard

5 DESIGN WAYZATA BOULEVARD

- Design options for Wayzata Boulevard
- **Activity:** Design your own Wayzata Boulevard for each segment

6 TRANSPORTATION & STREETScape IMPROVEMENTS

- Overview of improvement possibilities
- **Activity:** Identify the investment types that should be considered along the four segments of the Wayzata Boulevard study area.

7 NEXT STEPS

- Stakeholder Conversations
- CSC Meeting #6 – Implementation Discussion – May 20th

MEETING SUMMARY

The fifth meeting of the Wayzata Boulevard Corridor Study Committee (CSC) was held on April 20, 2023. The two-hour charrette was held surrounding the topics of transportation, mobility, safety, and streetscape. The presentation included a brief update of the project status, and most of the meeting was focused on activities to design Wayzata Boulevard. A copy of the presentation slides is attached after a summary of the activities.

01 Introductions

Discussion

- All CSC participants drive the Wayzata Boulevard corridor. Some attendees also walk and bike along the corridor for various reasons.

02 Project Updates

Discussion

- The last CSC meeting focused on land use opportunities and is being used to drive analysis and findings.
- The engagement team held pop-up events in February and an Open House in March to engage the community.
- Future CSC Meetings:
 - o CSC #6 – May 18
 - o CSC #7 – June TBD
 - o Final Plan Action - July

03 Purpose and Outcome

Discussion

- The transportation charette is designed to focus on roadway design, spot improvements, and streetscape design.
- Through the activities, CSC members should consider:
 - o Transportation Network; Adjacent Land Uses, Safety Improvements; Access Modifications; Roadway Design Changes; Identity and Sense of Place

04 Existing Conditions Overview

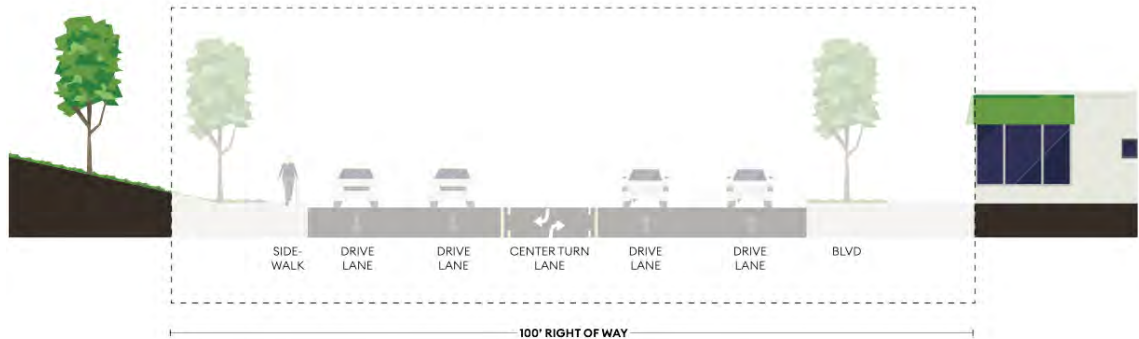
Discussion

- The team reviewed the existing roadway design and conditions of each segment of Wayzata Boulevard – including speed, number of lanes, multi-modal infrastructure, and traffic volumes.
- Example case study roadways were also provided with the agenda for review and consideration.

05 Design Wayzata Boulevard

Discussion

- The CSC members and project team were divided into three groups to create their own design for each segment of Wayzata Boulevard. Each group was given an existing cross section of the roadway and various pieces to represent design options (e.g., driving lanes, bike lanes, vegetation, etc.). The resulting designs and ideas are summarized by section below.



The following designs were created within the activity and include the design features identified (as applicable):

Design 1

| | |
|------------------------|---|
| Sidewalks | 6' sidewalk on both sides |
| Bike Infrastructure | N/A |
| Boulevard and Features | 10' Boulevard on both sides |
| Drive Lanes | Two 12' Driving Lanes in each direction |
| Center Lane/Boulevard | 12' Boulevard with left-turn lanes throughout |
| Notes: | -- |

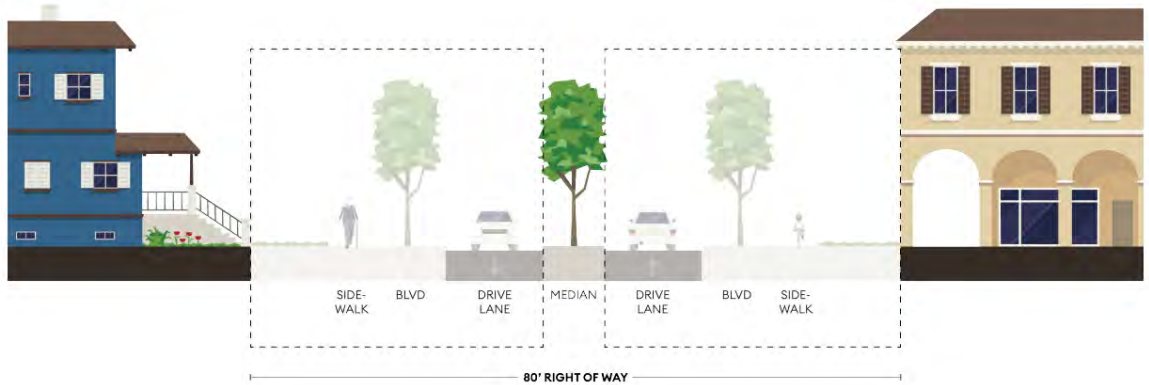
Design 2

| | |
|------------------------|--|
| Sidewalks | 6' sidewalk on both sides |
| Bike Infrastructure | N/A |
| Boulevard and Features | 10' Boulevard on both sides with lighting |
| Drive Lanes | Two 11' Driving Lanes in each direction |
| Center Lane/Boulevard | 12' Boulevard OR Center Turn Lane |
| Notes: | Reduce width of driving lanes to reduce speed. |

Design 3

| | |
|------------------------|---|
| Sidewalks | 10' sidewalks on both sides with furnishing zone (benches, bike rack, etc.) on one side |
| Bike Infrastructure | None |
| Boulevard and Features | 10' Boulevard on both sides |
| Drive Lanes | Two 11' Driving Lanes in each direction |
| Center Lane/Boulevard | Wide planted median |
| Notes: | -- |

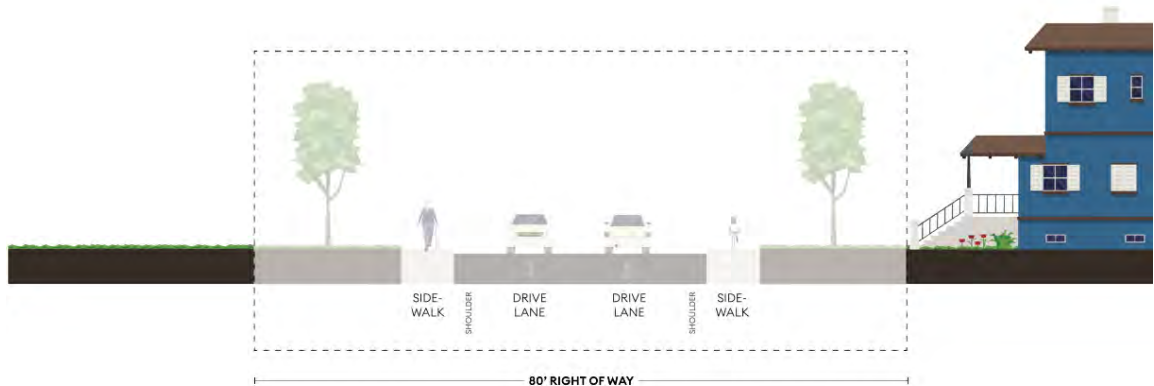
Wayzata
Blvd -
Central to
Bushaway



**Wayzata
Blvd –
Minnetonka
to Central**

One table provided specific design recommendations for this segment, as noted below. Other tables commented about the height of plantings in the median, and suggested maintaining the design as it is today.

| Design 1 | |
|------------------------|--|
| Sidewalks | 6' sidewalk on both sides |
| Bike Infrastructure | None |
| Boulevard and Features | 8' Boulevard on both sides |
| Drive Lanes | One 11' driving lane in each direction with on-street parking on either side |
| Center Lane/Boulevard | 10' Boulevard |
| Notes: | -- |



**Wayzata
Blvd –
Highway 12
to
Minnetonka**

The following designs were created within the activity and include the design features identified (as applicable):

| Design 1 | |
|------------------------|--|
| Sidewalks | 6' sidewalk on both sides |
| Bike Infrastructure | 2-way raised bikeway on the south side |
| Boulevard and Features | 8' Boulevard on both sides |
| Drive Lanes | One 11' driving lane in each direction |
| Center Lane/Boulevard | 6' Boulevard OR center turn lane |
| Notes: | -- |

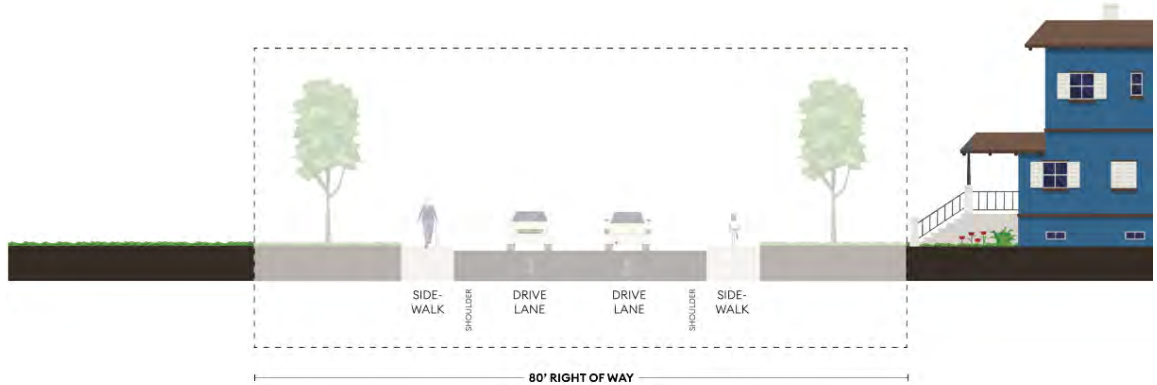
Design 2

| | |
|------------------------|--|
| Sidewalks | 8' sidewalk on both sides |
| Bike Infrastructure | Protected bike lane on both sides |
| Boulevard and Features | 6' Boulevard on both sides |
| Drive Lanes | One 12' driving lane in each direction |
| Center Lane/Boulevard | None |

Design 3

The group desired three improvements to this section:

- Increase tree canopy; Increase pedestrian connections; Consider roundabouts for Ferndale and/or Barry



The following designs were created within the activity and include the design features identified (as applicable):

Design 1

| | |
|------------------------|---|
| Sidewalks | 6' sidewalk on both sides |
| Bike Infrastructure | None |
| Boulevard and Features | 8' Boulevard on both sides |
| Drive Lanes | Two 12' driving lanes in each direction |
| Center Lane/Boulevard | None |

Design 2

| | |
|------------------------|---|
| Sidewalks | Sidewalk on both sides |
| Bike Infrastructure | None |
| Boulevard and Features | Boulevard on both sides |
| Drive Lanes | Two driving lanes in each direction |
| Center Lane/Boulevard | None |
| Notes: | Add greenspace and remove the south central to Colonial Square. |

Design 3

| | |
|------------------------|--|
| Sidewalks | 6' Sidewalk on both sides |
| Bike Infrastructure | None |
| Boulevard and Features | 10-12' Boulevard on both sides |
| Drive Lanes | One 11' driving lane in each direction |
| Center Lane/Boulevard | Center Turn Lane and/or Planted Median |

**Central Ave
– Highway
12 to
Wayzata**

06 Transportation & Streetscape Improvements

Each group was also provided with an aerial to identify specific spot improvements or ideas throughout the study area. The follow summarizes the discussion by section or location. Discussion items that were mentioned more than once are identified with a (#) at the end to represent the number of appearances.

Discussion

- **Wayzata Boulevard: Highway 12 to Minnetonka**
 - Gateway to Wayzata is at the intersection with Ferndale. (2)
 - Explore a potential interchange at Ferndale and Highway 12. (2)
 - Intersection improvements needed at Barry Avenue and Wayzata Boulevard – signal, refuge, etc.
 - Close sidewalk gap west of Minnetonka Avenue on south side.
 - Prioritize bike connection between Barry Avenue and Ferndale Road (consider on-street protected bike lane.
 - Consider a roundabout at Ferndale Road and Wayzata Boulevard.
 - Provide a trail between Barry Avenue and Ferndale Road.
 - Extend the raised median design to Barry Avenue
 - Consider a Flashing Beacon at Minnetonka Avenue and Wayzata Boulevard.
 - Consider a roundabout at Barry Avenue and Wayzata Boulevard.
- **Wayzata Boulevard: Minnetonka to Central**
 - Complete sidewalk improvements as needed.
- **Wayzata Boulevard: Central to Bushaway**
 - There are pedestrian issues between Benton Avenue and Central Avenue for crossing Wayzata Blvd and navigating the intersections.
 - It is hard to take a left onto Wayzata Boulevard from Benton Avenue.
 - Making a left onto Wayzata Boulevard from the Colonial Square access is challenging.
 - There are too many access points between Birch Bend Lane and Bushaway Road that result in many questionable left turns.
 - Provide mid-block crossings with a flashing beacon at two locations along Wayzata Boulevard. Consider roundabouts at these locations to facilitate U-turns for drivers.
 - Gateway to Wayzata is at the intersection with Bushaway Road.
- **Central Avenue: Highway 12 to Wayzata**
 - Close southern access to Huntington Bank/Colonial Square. (2)
 - Explore U-turn options for northbound to southbound movement.
 - Left turns from SB Wayzata Boulevard to the norther Colonial Square access are difficult. Could it be improved by adding a turn lane and reducing the northbound movement to one lane in this section?
 - Consider a three-lane section with sidewalks.



WAYZATA BOULEVARD CORRIDOR STUDY CORRIDOR STUDY COMMITTEE #5

April 20, 2023





Agenda

- 1** Introductions
- 2** Project Updates
- 3** Purpose and Outcome
- 4** Existing Conditions Overview
- 5** Design Wayzata Boulevard
- 6** Transportation & Streetscape Improvements
- 7** Next Steps

Introductions

- Name
- Council/Committee Representation
- Ice Breaker: How do you currently move along Wayzata Boulevard?



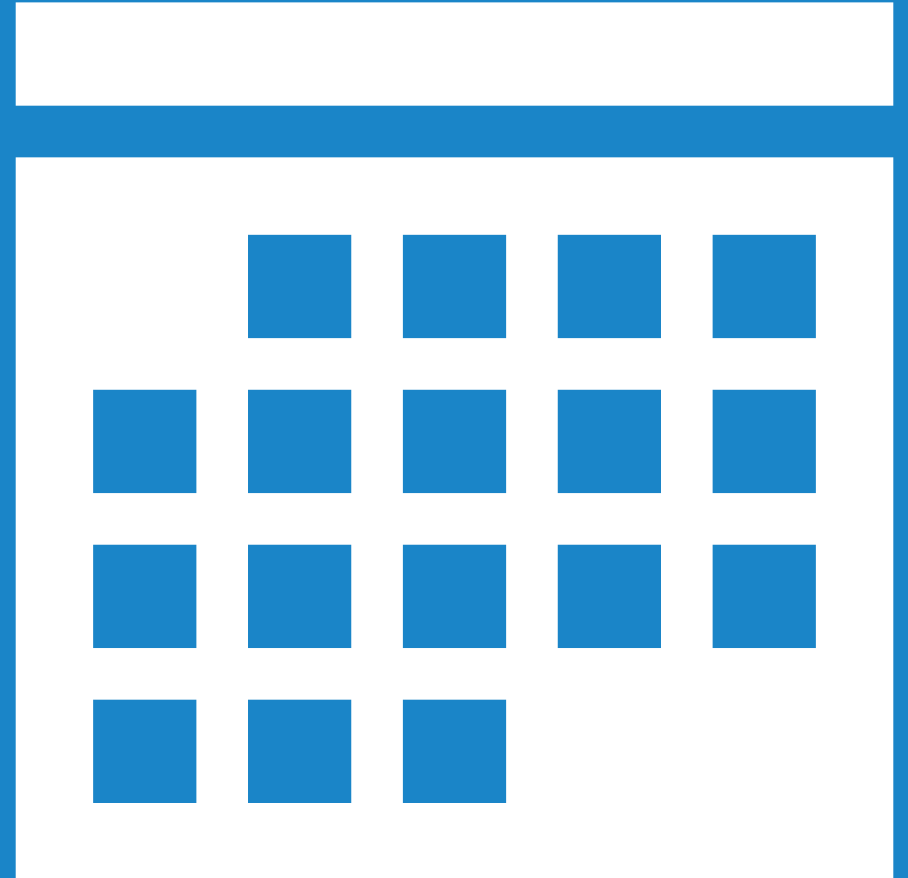
Project Updates



Project Updates

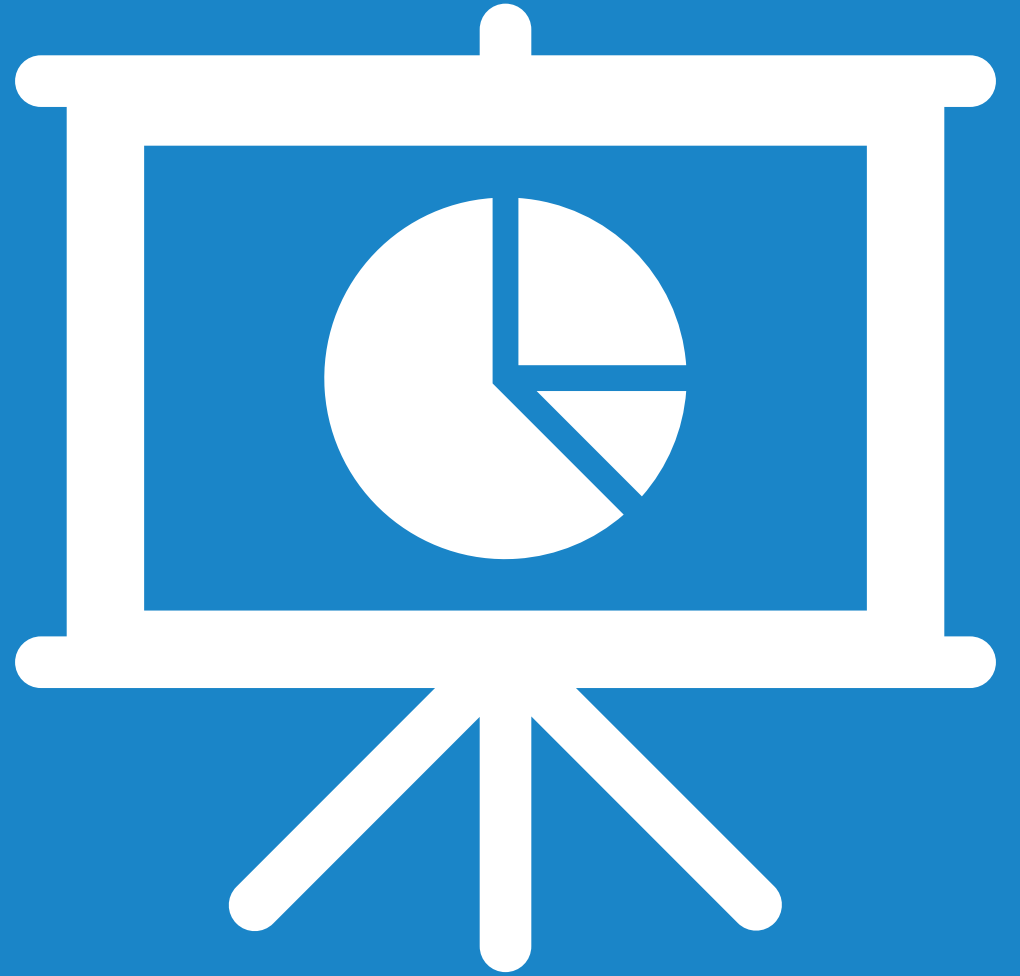
- CSC Meeting #5 – Land Use
- February Pop-Up Events
- March Open House

- Schedule Update
 - CSC #6 – May 18
 - CSC #7 – June
 - Final Plan Action - July



Purpose and Outcome

- What is the intent of the Transportation Charrette?
- What is the desired outcome?



Purpose and Outcome

- 
- **Roadway Design**
 - **Spot Improvements**
 - **Streetscape Design**

Consider:

- Transportation Network
- Adjacent Land Uses
- Safety Improvements
- Access Modifications
- Roadway Design Changes
- Identity and Sense of Place

Existing Conditions Overview

Transportation Characteristics
and Conditions

- Speed
- # of Lanes
- On-Street Parking
- Multi-modal Infrastructure
- Sidewalks and Trails
- Volumes



Wayzata Blvd – Central to Bushaway



Wayzata Blvd - Minnetonka to Central



Wayzata Blvd - Highway 12 to Minnetonka



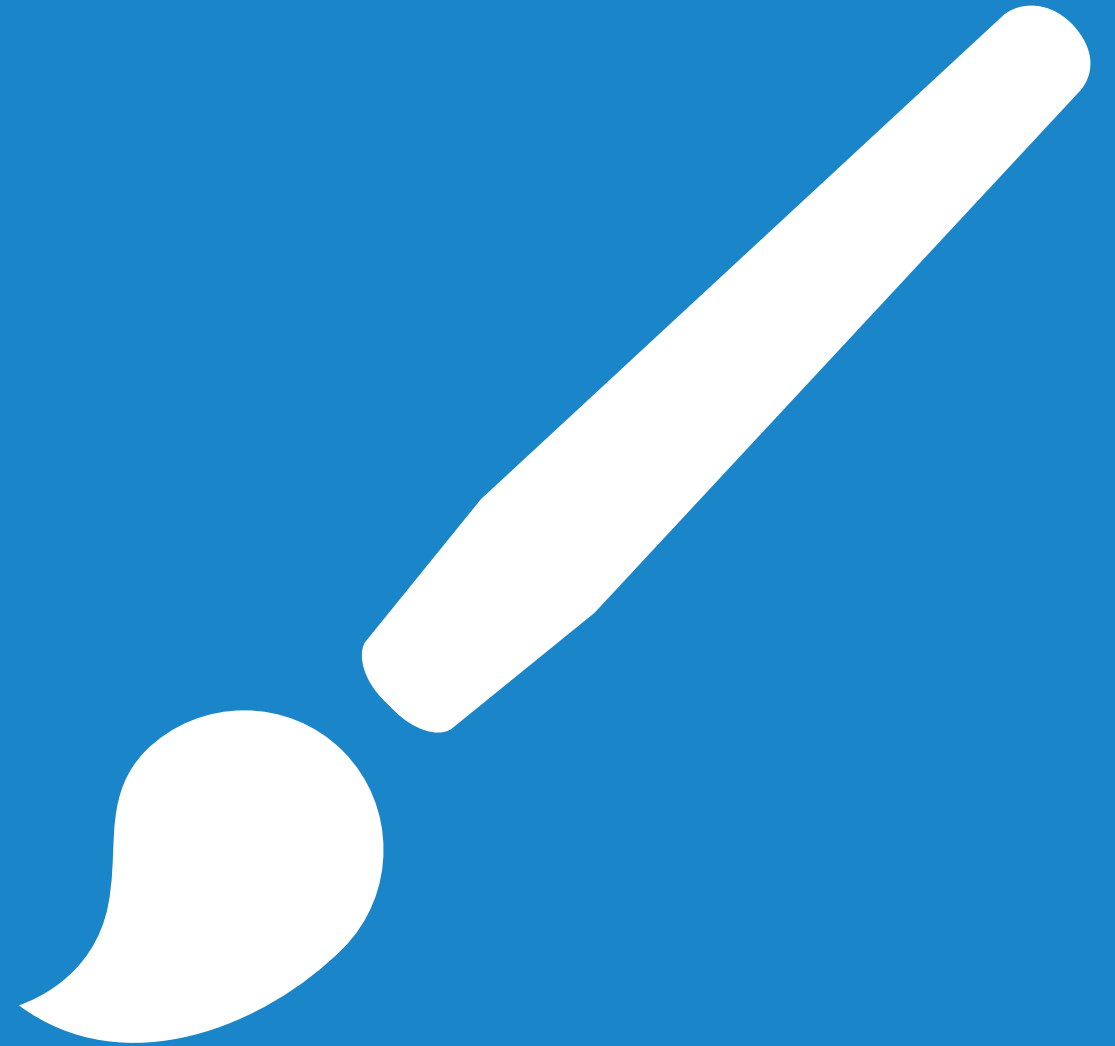
Central Ave - Wayzata Blvd to Highway 12



Design Wayzata Boulevard

ACTIVITY

- Create your own design for each segment of Wayzata Boulevard
 - Driving lanes
 - Sidewalks
 - Bike Lanes
 - Vegetation
 - Adjacent Development



Transportation & Streetscape

ACTIVITY

- What are the spot improvements or considerations we need to address?
- Where are there opportunities to showcase the Wayzata charm and sense of place?



Next Steps

- Meeting #6: Implementation Discussion – May 18
- Stakeholder Conversations
- Corridor Study Development



APPENDIX C: REFERENCES

Appendix C: References

The following documents are referenced throughout the corridor study and are appended by reference.

Wayzata Design Standards Handbook

City of Wayzata Wayfinding Plan

Wayzata 2040 Comprehensive Plan

City of Wayzata Strategic Plan 2019-2023

Hennepin County Capital Improvement Program